

INDEX OF SHEETS

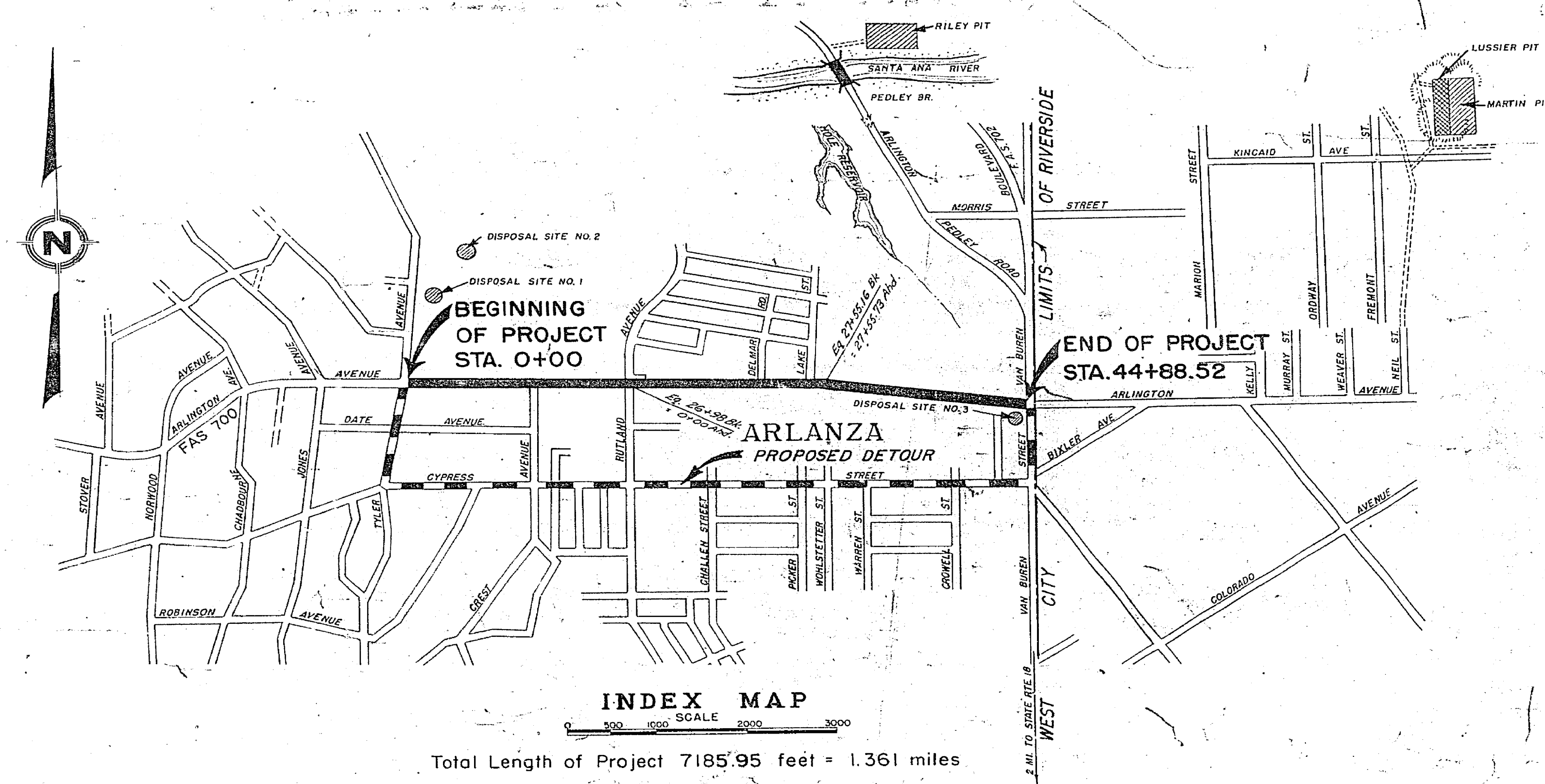
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" 3-5	Plan & Profile
" 6	Intersection Details
" 7-8	Construction Details
" 9-13	Standard Structures
" 1-22	Cross Sections

STATE OF CALIFORNIA
 DEPARTMENT OF PUBLIC WORKS
 DIVISION OF HIGHWAYS
 COUNTY OF RIVERSIDE
 PLAN AND PROFILE

FEDERAL AID SECONDARY PROJECT
 NO. S-700 (I)

ON ARLINGTON AVENUE BETWEEN TYLER AVENUE AND VAN BUREN STREET

DIST.	COUNTY	ROUTE	SEC.	SHEET NO.	TOTAL SHEETS	FED. AID DIV. NO.	STATE	PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
VIII	RIV.	F. A. S.	700	1	13		CAL.	S-700 (I)	1954	1	13



INDEX MAP
 SCALE 0 500 1000 2000 3000
 Total Length of Project 7185.95 feet = 1.361 miles

CONVENTIONAL SIGNS

COUNTY LINE	RAILROAD TRACKS
CITY OR TOWN LIMITS	SOLVENTS
TOWNSHIP LINE	DROP INLET
GRANT LINE	POWER POLE
SECTION LINE	POWER TOWER
FENCE	TELEGRAPH OR TELEPHONE POLE
GUARD RAIL	MARSH
UNFENCED PROPERTY	RIGHT OF WAY FENCE TO BE CONSTRUCTED
RIGHT OF WAY LINE	
BASE OR SURVEY LINE	
TRAVELED WAY	

COUNTY OF RIVERSIDE
 COUNTY SURVEYOR
 ROAD COMMISSIONER
 CIVIL ENGINEER LICENSE NUMBER 4398
 APPROVED BY THE BOARD OF SUPERVISORS
 3/13/1954
 BY *[Signature]* CHAIRMAN
 STATE OF CALIFORNIA
 DISTRICT ENGINEER DISTRICT VIII
 APPROVED *[Signature]* 1/14/1954
 STATE HIGHWAY ENGINEER
 CIVIL ENGINEER LICENSE NUMBER
 DEPT. OF COMMERCE
 BUREAU OF PUBLIC ROADS
 RECOMMENDED FOR APPROVAL
 DISTRICT ENGINEER
 DEPT. OF COMMERCE
 BUREAU OF PUBLIC ROADS
 APPROVED
 DIVISION ENGINEER
 DEPT. OF COMMERCE
 BUREAU OF PUBLIC ROADS

65-8003-P R-120

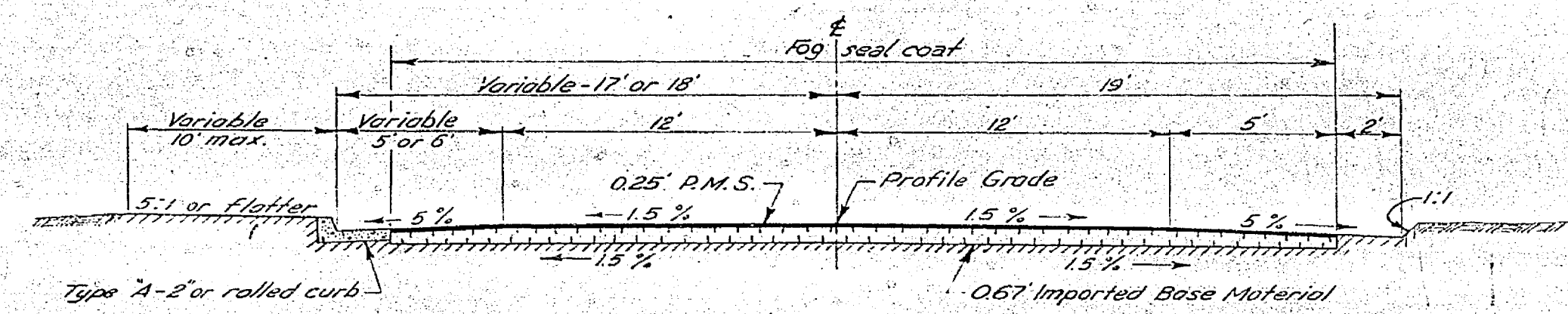
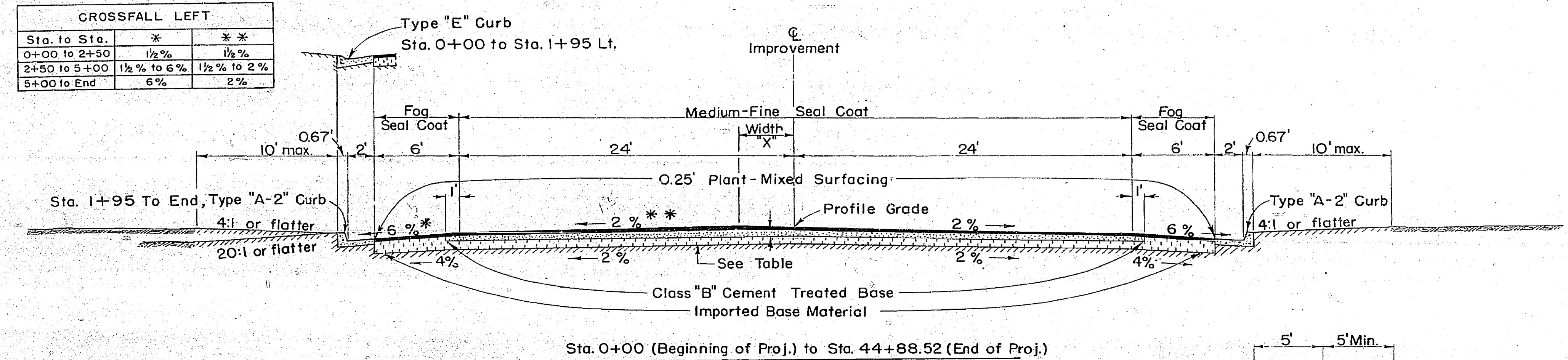
FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
2	CAL.	S 700(1)	1955	2	13

DIST.	COUNTY	ROUTE	SEC.	SHEET NO.	TOTAL SHEETS
VIII	RIV.	F.A.S. 700	2	2	13

Approved: *[Signature]*
 Dist. Eng. Dist. VIII
[Signature]
 Engineer, Surveys and Plans
[Signature]
 County Road Commissioner

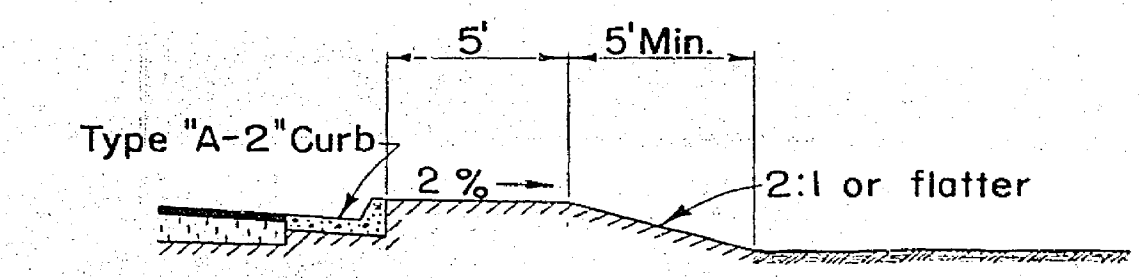
Design Classification: 1900-74-M-50
 E. W. L. = 2,600,000

Sta. to Sta.	*	**
0+00 to 2+50	1 1/2 %	1 1/2 %
2+50 to 5+00	1 1/2 % to 6 %	1 1/2 % to 2 %
5+00 to End	6 %	2 %



TYPICAL SECTION CROSS ROADS

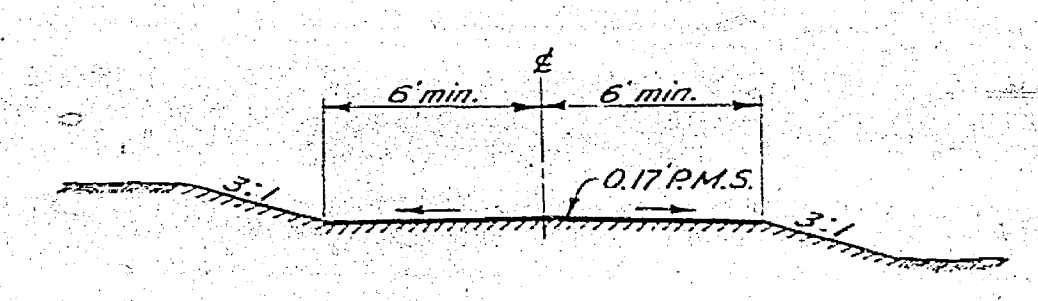
STATION TO STATION	WIDTH "X"
0+00 to 2+50	12'
2+50 to 5+00	12'-0"
5+00 to 44+88.52	0'



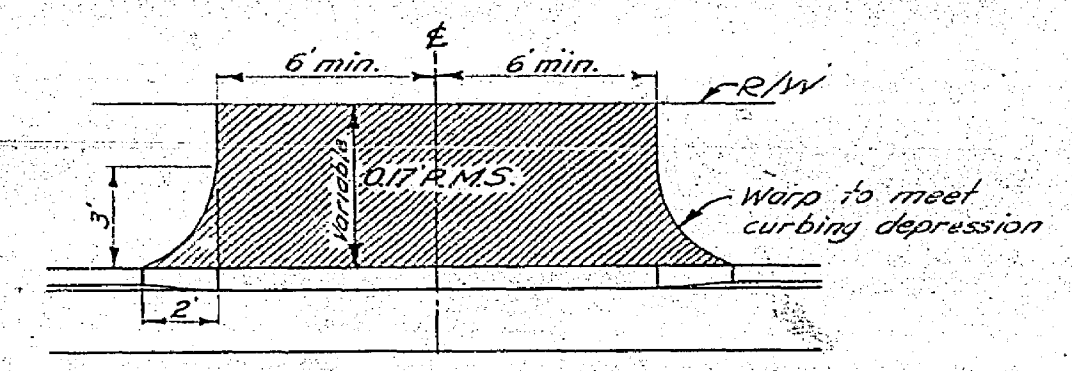
Sta. 27+00 to Sta. 34+00 (Left)

IMPORTED BASE MATERIAL
(includes C.T.B. portions)

STATION TO STATION	THICKNESS
0+00 to 28+00 (east)	1.0'
28+00 (east) to 44+88.52	0.67' - 1.0'



TYPICAL SECTION DRIVEWAYS
(No Scale)



TYPICAL PLAN DRIVEWAYS
(No Scale)

Note: All driveway pavement dimensions variable to suit existing access conditions.

(VIII - Riv - 700)
TYPICAL ROAD SECTIONS
ARLINGTON AVENUE

Scale: 1" = 5'

Superelevation as directed by the Engineer.

Recommended: *[Signature]*
 Asst. State Highway Engineer
[Signature]
 Engineer of Design
 License No. **R-1120**

55-8DC3-P

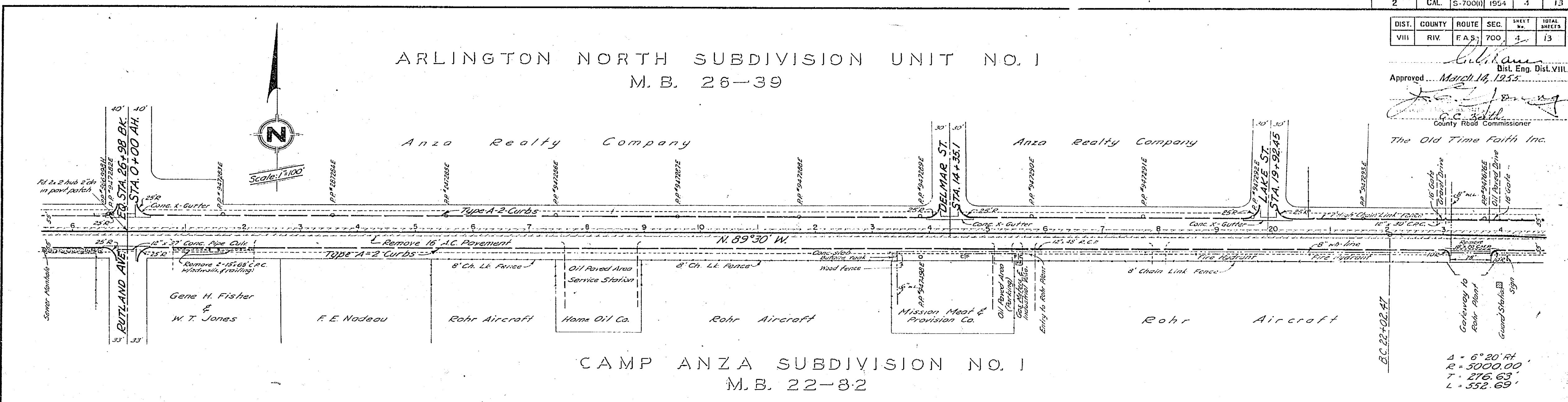
324

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
2	CAL.	S-700N1	1954	4	13

DIST.	COUNTY	ROUTE	SEC.	SHEET NO.	TOTAL SHEETS
VIII	RIV.	F.A.S.	700	4	13

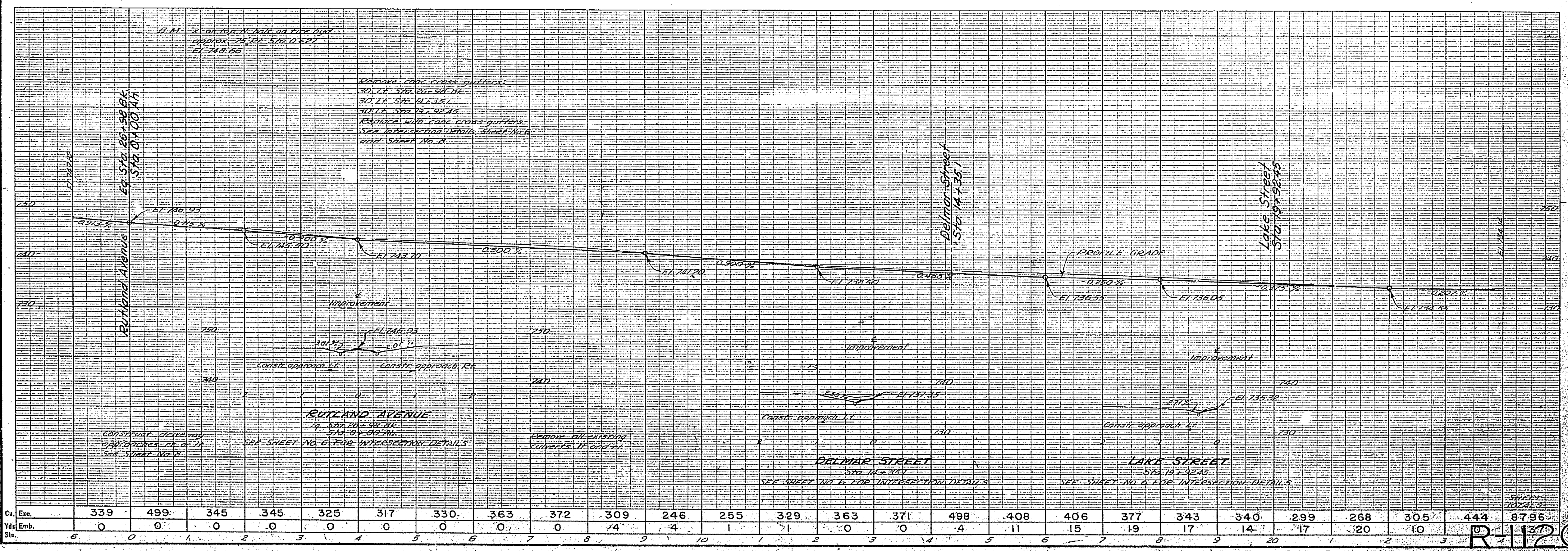
Approved: *[Signature]*
 March 16, 1955
 Dist. Eng. Dist. VIII.
[Signature]
 County Road Commissioner

ARLINGTON NORTH SUBDIVISION UNIT NO. 1
 M.B. 26-39



CAMP ANZA SUBDIVISION NO. 1
 M.B. 22-82

Δ = 6° 20' Rt
 R = 5000.00
 T = 276.63
 L = 552.69'



Cu. Exc.	339	499	345	345	325	317	330	363	372	309	246	255	329	363	371	498	408	406	377	343	340	299	268	305	444	87.96
Vol. Emb.	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Sta.	6	0	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24

55-8063-P

R-120

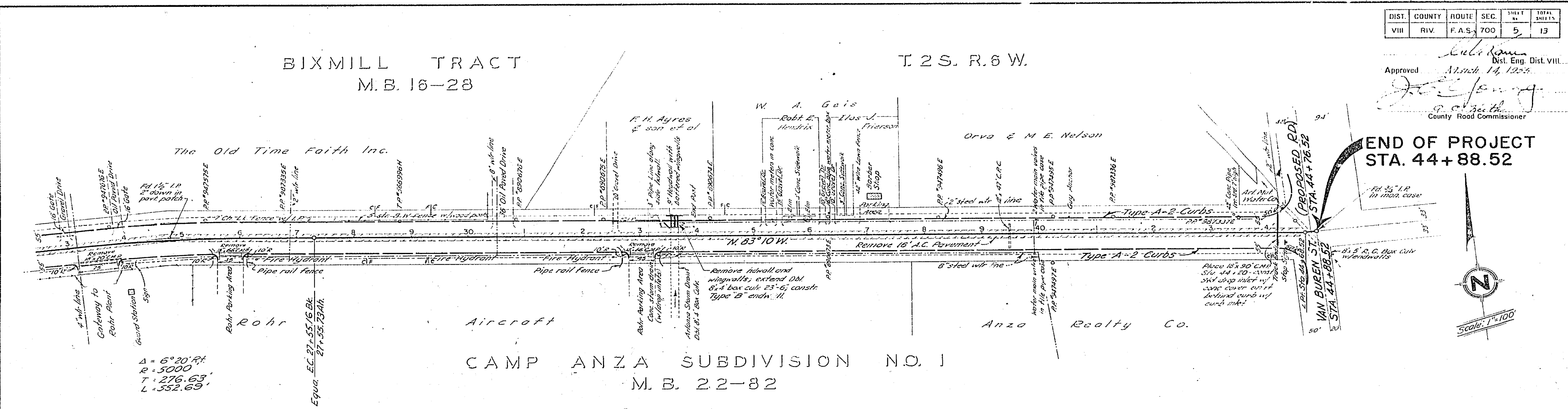
DIST.	COUNTY	ROUTE	SEC.	SHEET NO.	TOTAL SHEETS
2	CAL.	5-7000	1954	5	13

DIST.	COUNTY	ROUTE	SEC.	SHEET NO.	TOTAL SHEETS
VIII	RIV.	F.A.S.	700	5	13

Approved: *John A. ...*
 Dist. Eng. Dist. VIII
 County Road Commissioner

BIXMILL TRACT
 M.B. 16-28

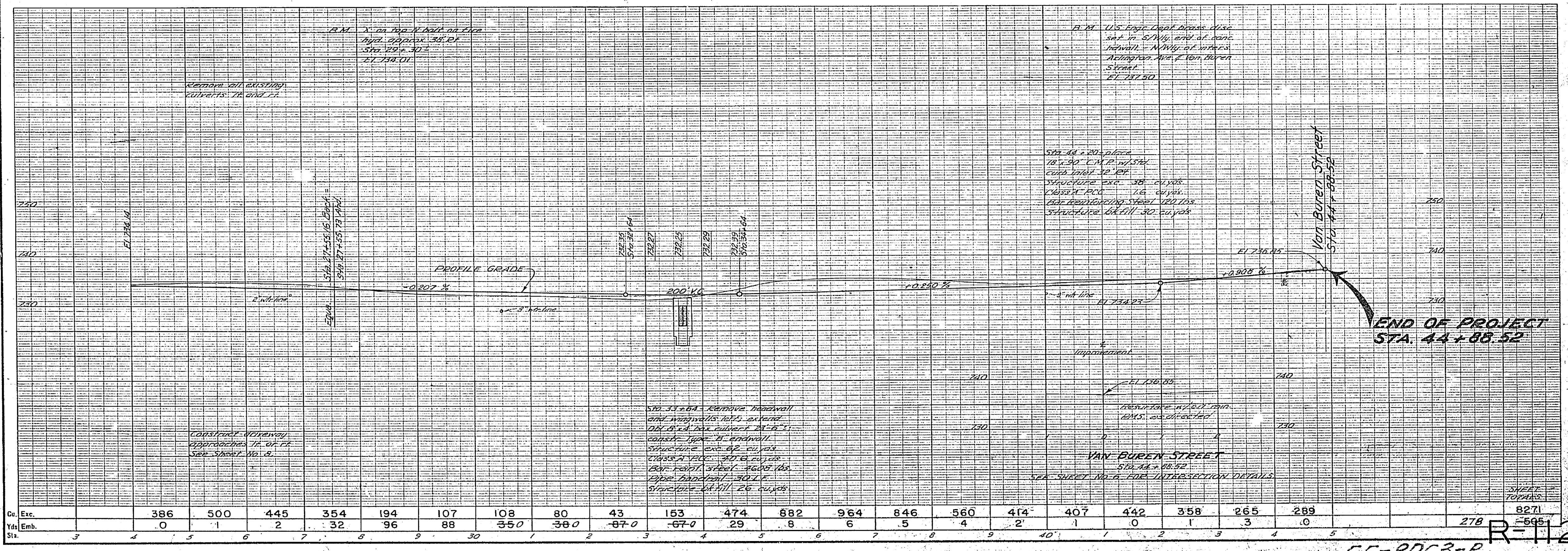
T 2 S. R. 6 W.



END OF PROJECT
 STA. 44+88.52

$\Delta = 6^\circ 20' 34''$
 $R = 5000'$
 $T = 276.63'$
 $L = 552.63'$

CAMP ANZA SUBDIVISION NO. 1
 M.B. 22-82

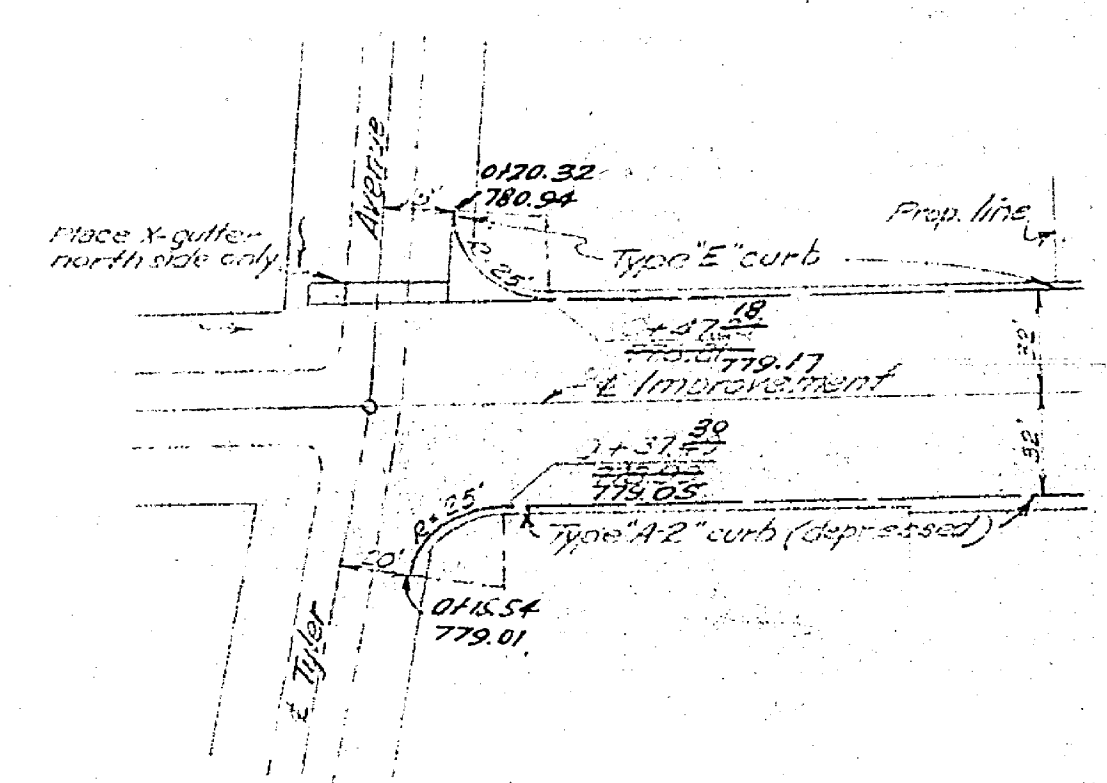


END OF PROJECT
 STA. 44+88.52

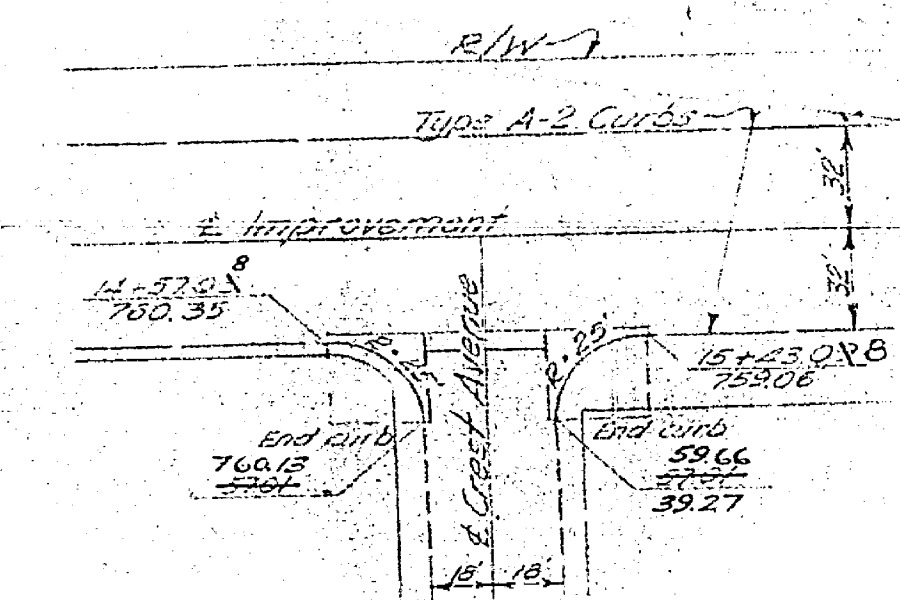
DIST. NO.	STATE	SECTION	PROJECT	SHEET	TOTAL SHEETS
8	GA.	15-1000	1955	6	13

DIST.	COUNTY	ROUTE	SEC.	SHEET NO.	TOTAL SHEETS
VIII	HW.	F.A.S.	700	6	13

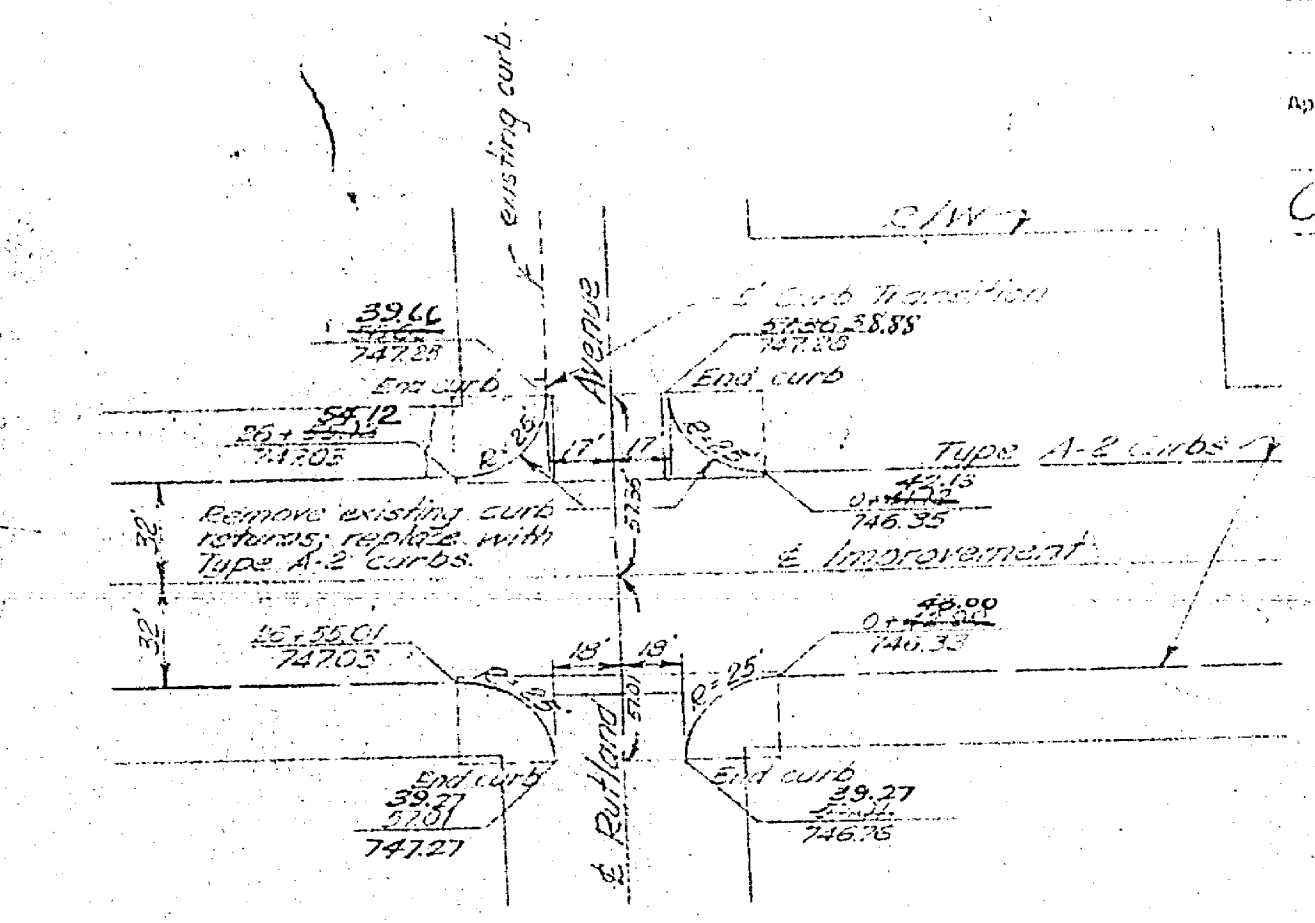
Approved: *J. C. Smith*
 J. C. Smith
 County Board Commissioner



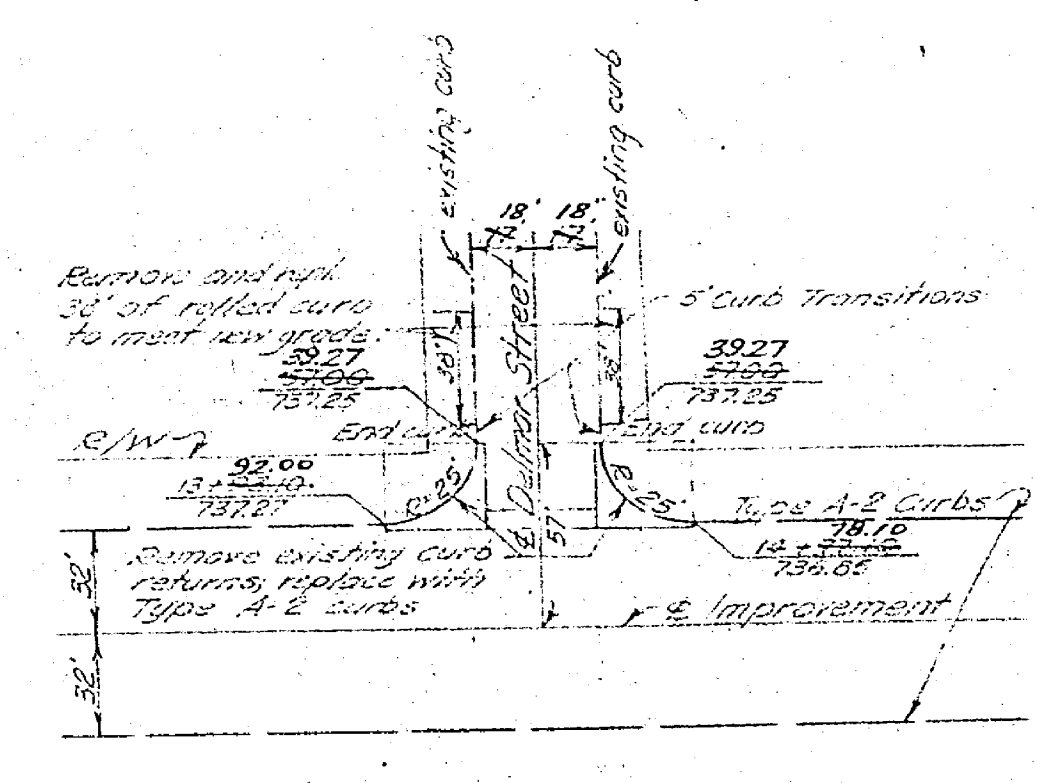
TYLER AVENUE INTERSECTION
 STA 0+00



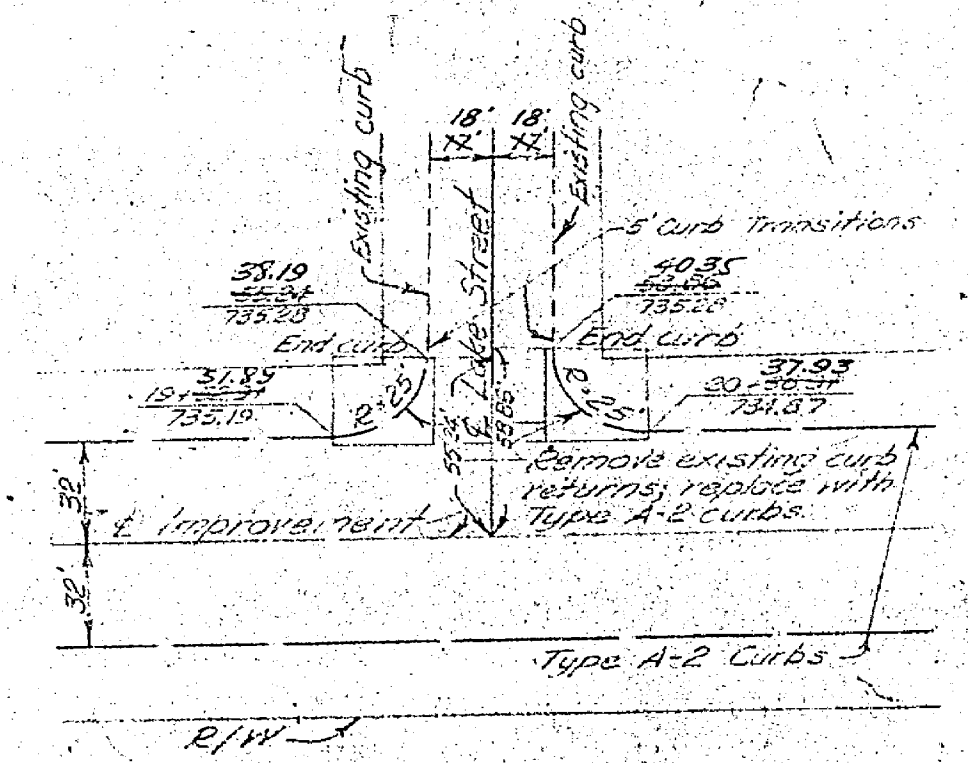
CREST AVENUE INTERSECTION
 STA 15+00.08



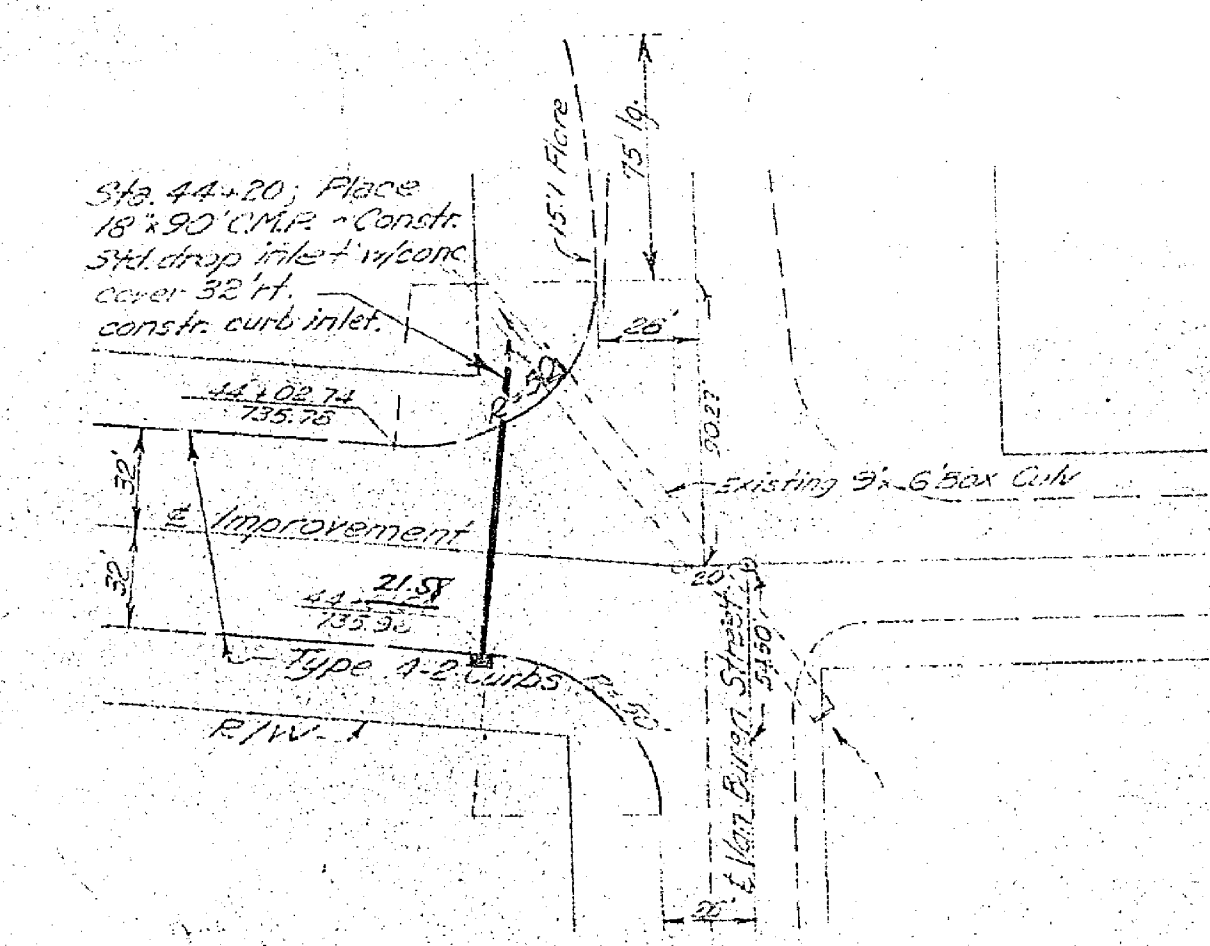
RUTLAND AVENUE INTERSECTION
 EQU. STA. 26+38 BK.
 STA. 0+00 AH.



DELMAR STREET INTERSECTION
 STA. 14+35.10



LAKE STREET INTERSECTION
 STA. 19+92.45



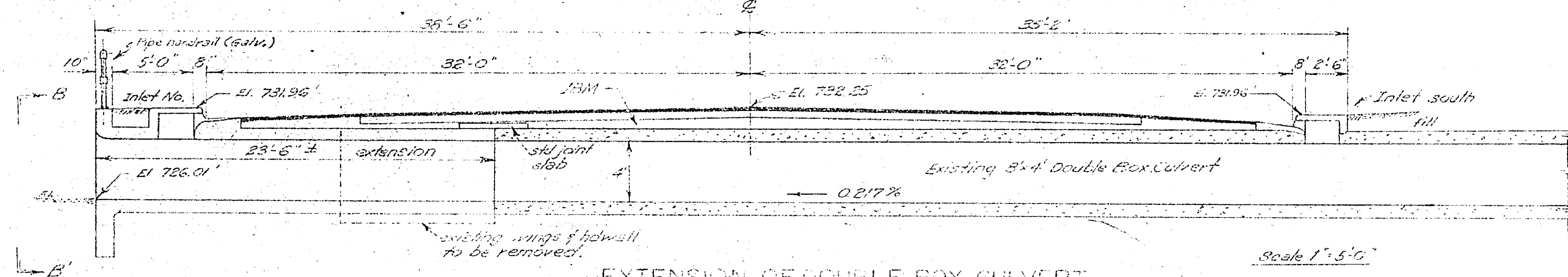
VAN BUREN STREET INTERSECTION
 STA. 44+88.52

Note: All elevations given
 to top of curb.

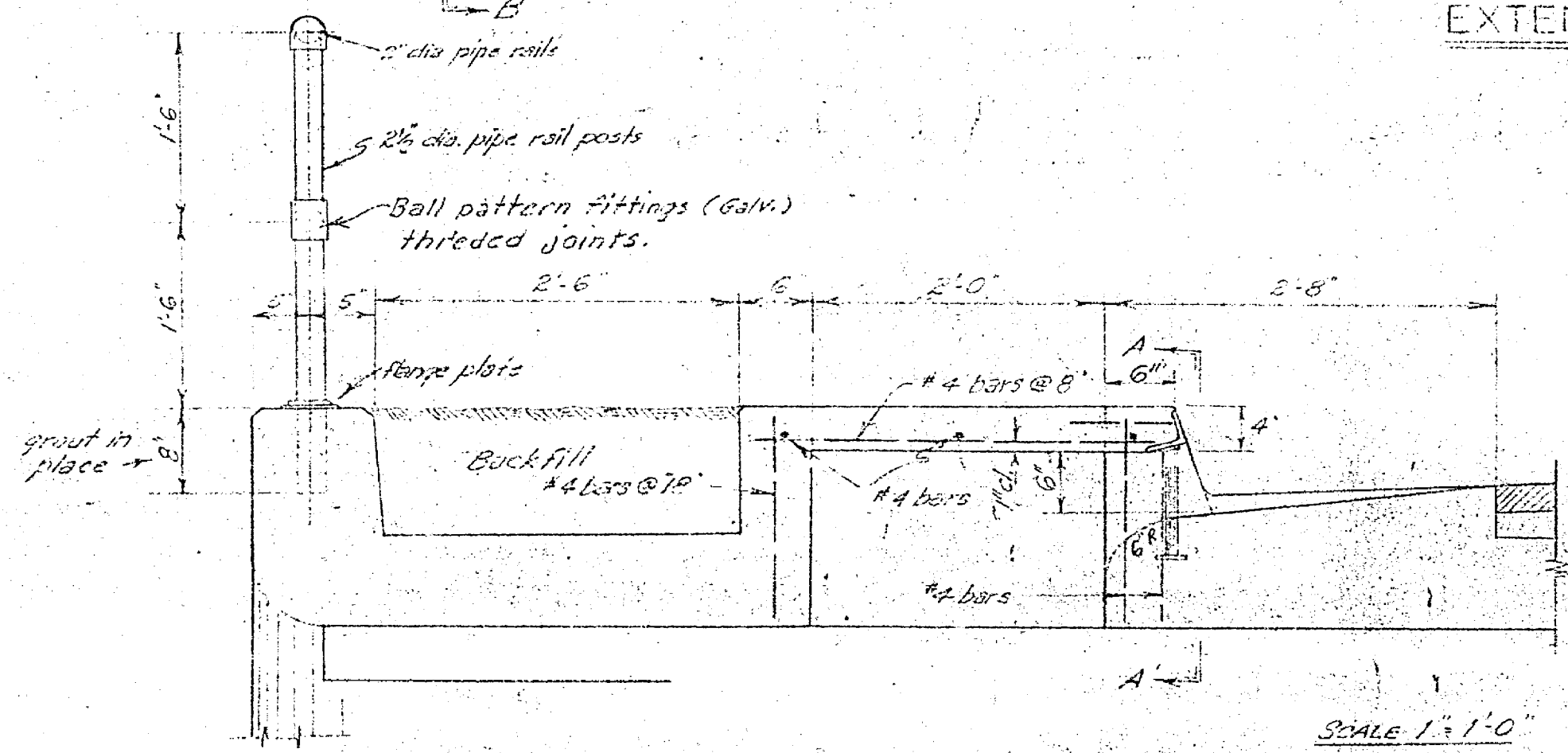
INTERSECTION DETAILS
 SCALE: 1" = 50'

NO.	DATE	BY	CHKD.	APP'D.	TOTAL
2	CAL	2/20/55			13
VIII	RIV	1955			13

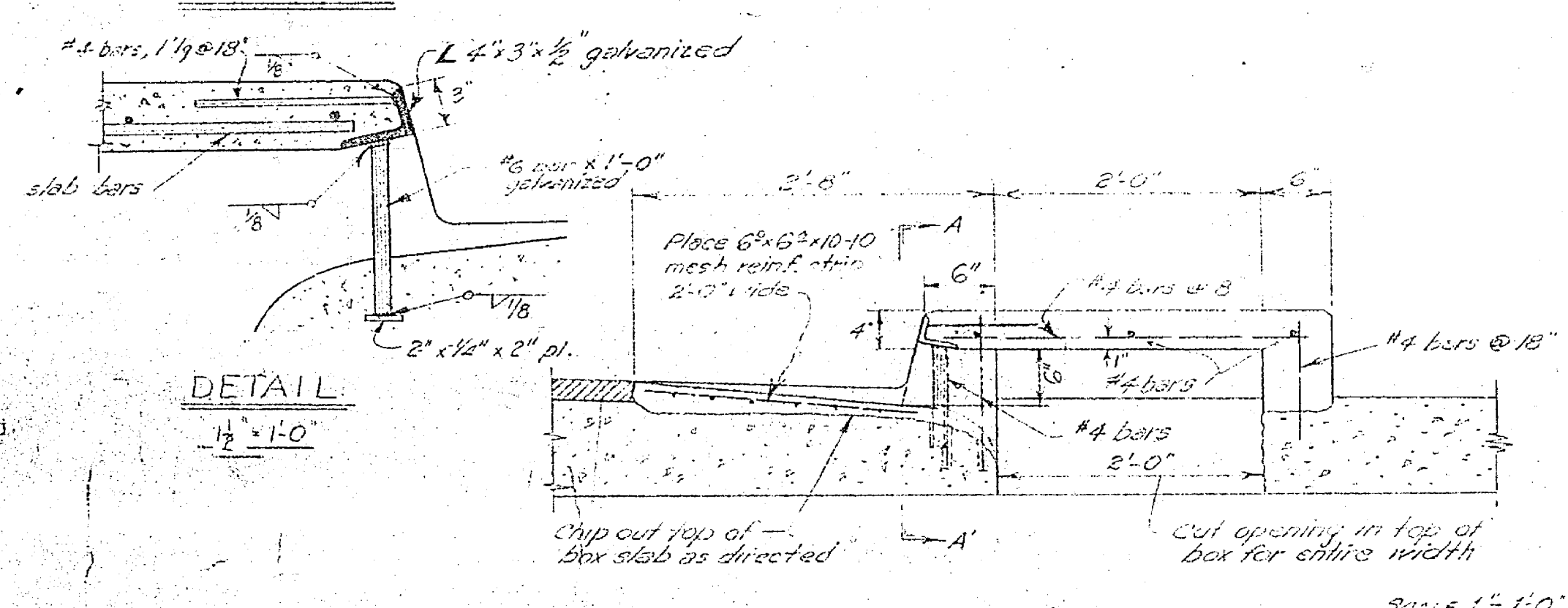
Approved: *L.H. Kamm*
 L.H. Kamm
 CIVIL ENGINEER
 1.4.1955
R. C. Smith
 CIVIL ENGINEER



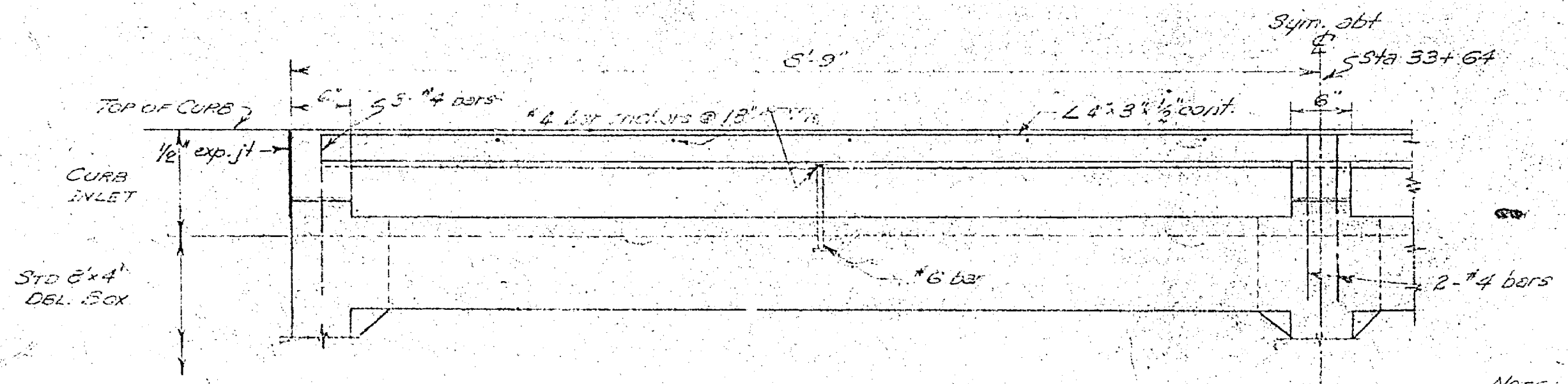
EXTENSION OF DOUBLE BOX CULVERT
STA 33+64



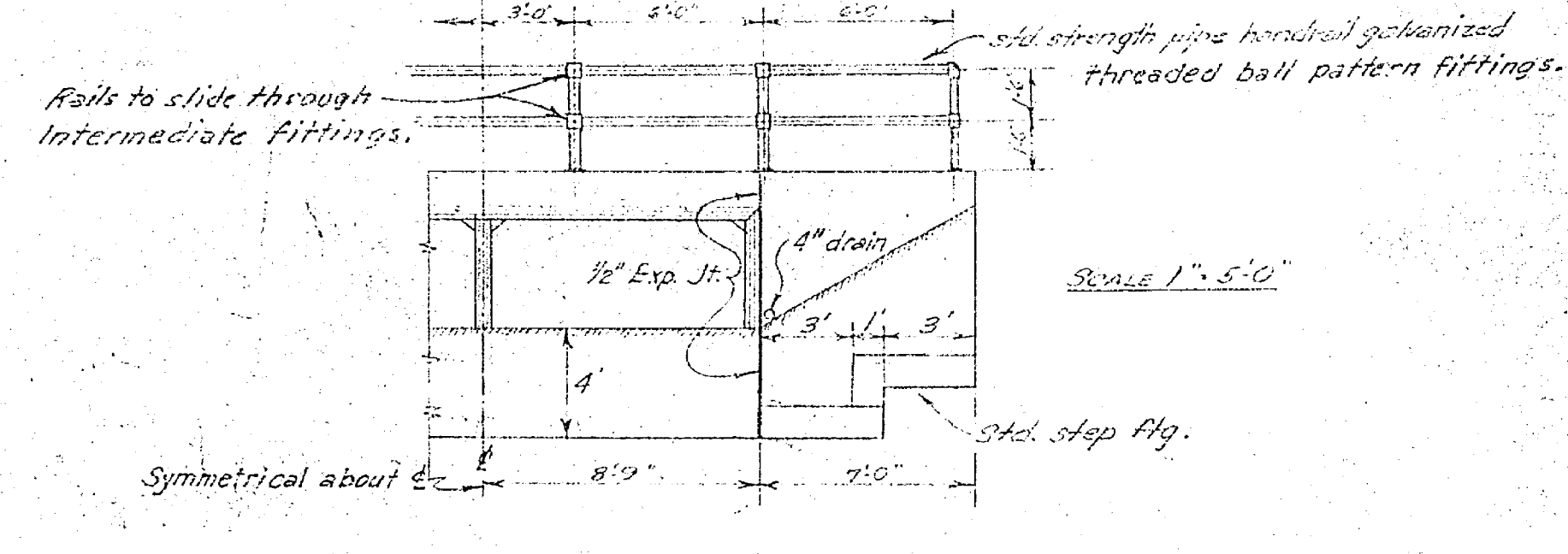
CURB INLET - NORTH



CURB INLET - SOUTH



INLET - VIEW A-A



HEADWALL - VIEW B-B

Note:
 See Standard Dbl Box Culvert
 sheet for details not shown.
 Dbl Box 8'-4" - A-18 x 23'-6"

CULVERT DETAILS

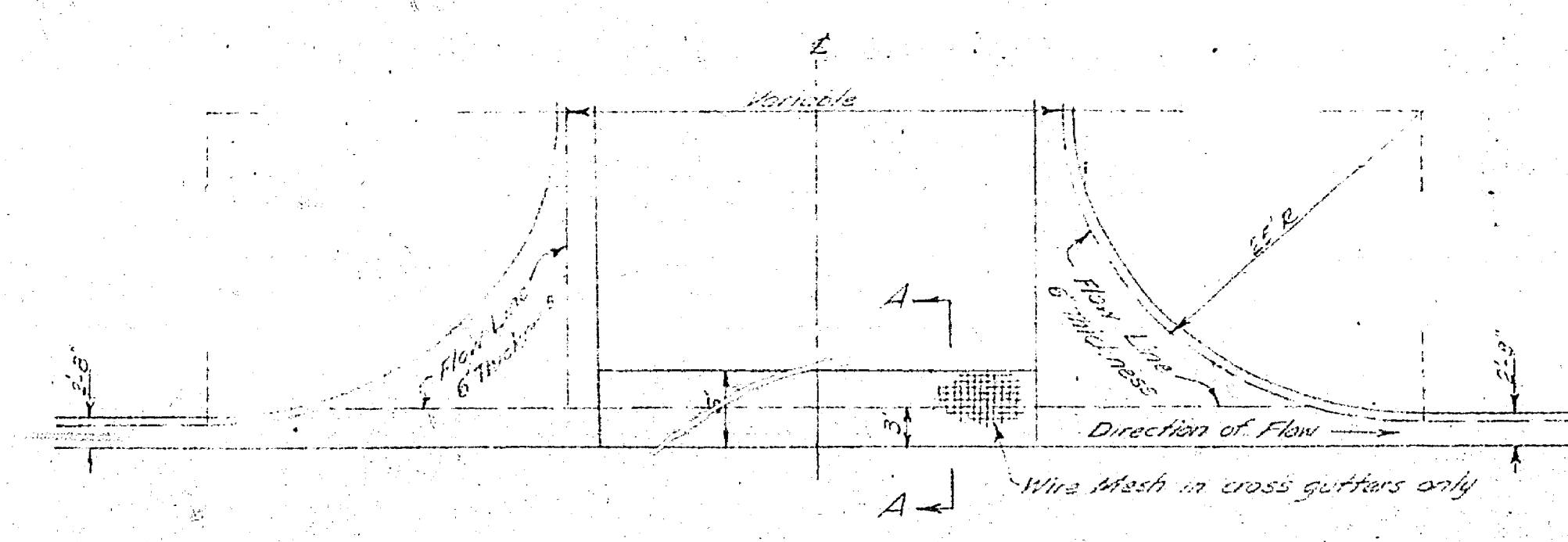
ARLINGTON AVENUE FAS-700(1)

55-8003-R-1120

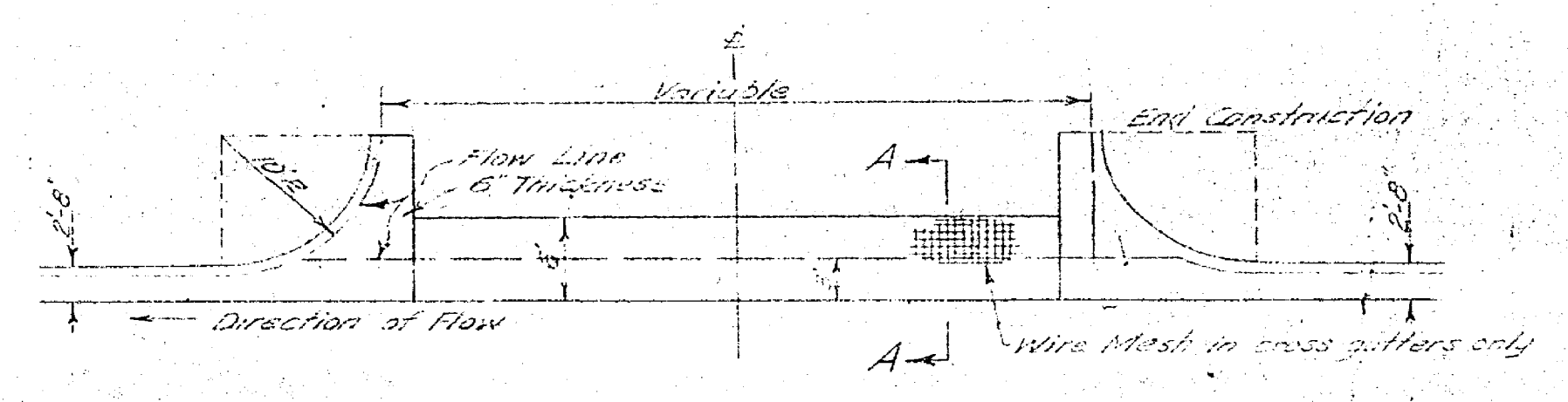
Structural Details
 Checked by: *John S. ...*
 Approved: *...*

DIST.	COUNTY	ROUTE	SEC.	POST MILE	STA. OFFSET
VIII	F.A.S.	700	8	13	

L.L. Kamm
 City Engineer, VIII
 1955
 J.C. Young
 County Road Commissioner

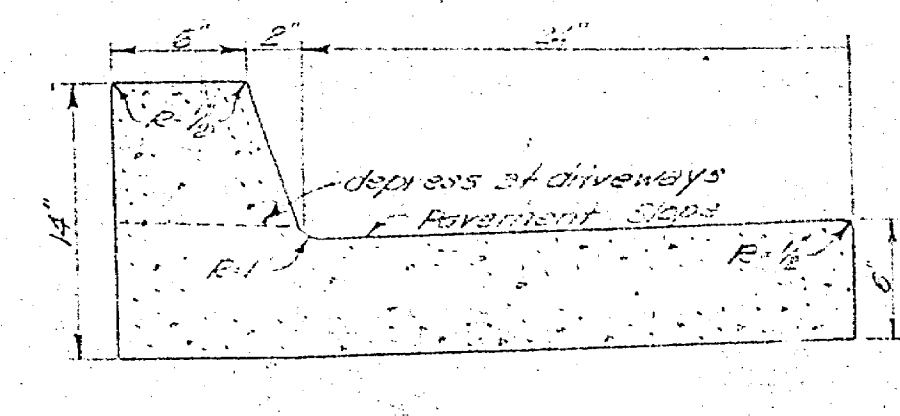


TYPICAL INTERSECTION AT CROSS ROAD



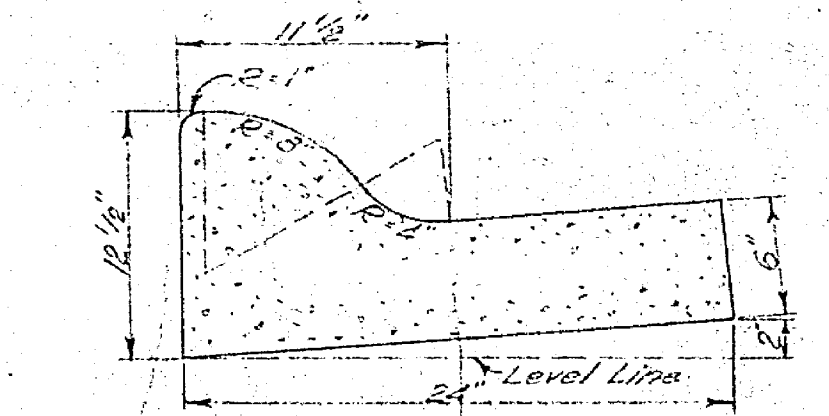
TYPICAL ENTRANCE TO ROLLER PARKING AREA

SCALE: 1" = 10'



TYPE A-2 CURB

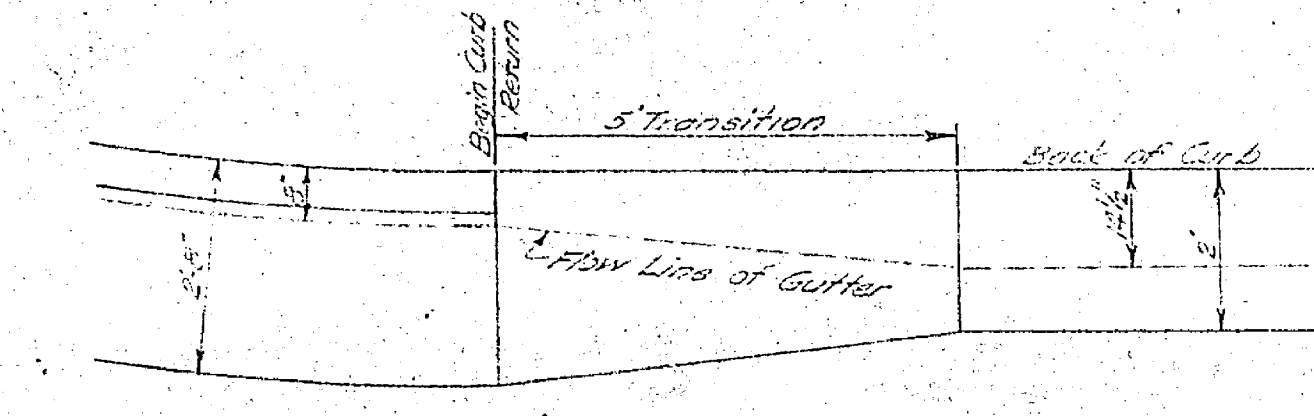
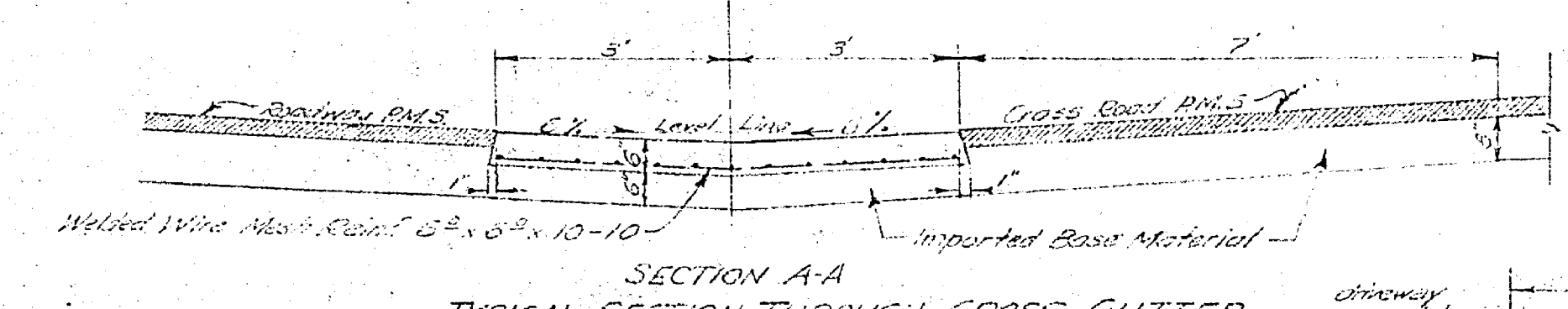
SCALE: 1 1/2" = 1'



TYPE ROLLED CURB

to be removed and replaced at Delmar Street Inters.

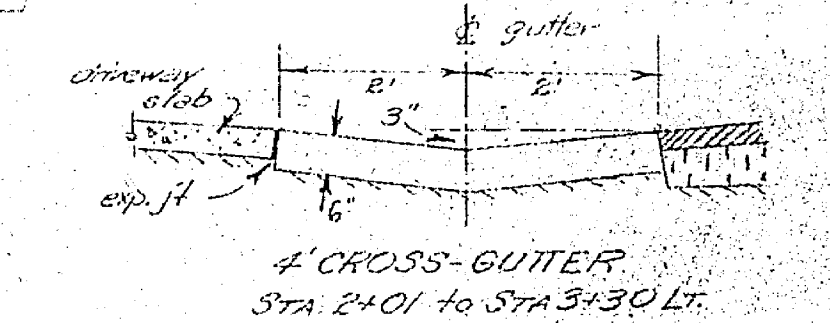
SCALE: 1 1/2" = 1'



ELEVATION

CURB AND GUTTER TRANSITION

SCALE: 1" = 2'



CONCRETE REMOVAL			
STATION	LT OR RT	DESCRIPTION	CU. YDS.
2+50	LT	Drive apron to pumps	2.2
2+51	LT	Drive apron	0.5
2+26	RT	Conc. catch basin	0.1
26+26.6	LT	Conc. X-gutter & curbs	3.8
14+35.1	LT	Conc. X-gutter & curbs	17.2
19+22.5	LT		13.6
13+23	RT	Conc. sidewalk	0.9
33+64	LT	Wings of sidewalk	2.0
Various Locations, Foundation Roadways, etc.			
TOTAL			62.4

DRIVEWAY APPROACHES					
Tulley Ave. to Rutland Ave.		Rutland Ave. to Ken Evers Street			
Station	Width	Station	Width	Station	Width
0+00	12'	0+00	12'	0+00	12'
0+15	12'	0+15	12'	0+15	12'
0+30	12'	0+30	12'	0+30	12'
0+45	12'	0+45	12'	0+45	12'
0+60	12'	0+60	12'	0+60	12'
0+75	12'	0+75	12'	0+75	12'
0+90	12'	0+90	12'	0+90	12'
0+105	12'	0+105	12'	0+105	12'
0+120	12'	0+120	12'	0+120	12'
0+135	12'	0+135	12'	0+135	12'
0+150	12'	0+150	12'	0+150	12'
0+165	12'	0+165	12'	0+165	12'
0+180	12'	0+180	12'	0+180	12'
0+195	12'	0+195	12'	0+195	12'
0+210	12'	0+210	12'	0+210	12'
0+225	12'	0+225	12'	0+225	12'
0+240	12'	0+240	12'	0+240	12'
0+255	12'	0+255	12'	0+255	12'
0+270	12'	0+270	12'	0+270	12'
0+285	12'	0+285	12'	0+285	12'
0+300	12'	0+300	12'	0+300	12'
0+315	12'	0+315	12'	0+315	12'
0+330	12'	0+330	12'	0+330	12'
0+345	12'	0+345	12'	0+345	12'
0+360	12'	0+360	12'	0+360	12'
0+375	12'	0+375	12'	0+375	12'
0+390	12'	0+390	12'	0+390	12'
0+405	12'	0+405	12'	0+405	12'
0+420	12'	0+420	12'	0+420	12'
0+435	12'	0+435	12'	0+435	12'
0+450	12'	0+450	12'	0+450	12'
0+465	12'	0+465	12'	0+465	12'
0+480	12'	0+480	12'	0+480	12'
0+495	12'	0+495	12'	0+495	12'
0+510	12'	0+510	12'	0+510	12'
0+525	12'	0+525	12'	0+525	12'
0+540	12'	0+540	12'	0+540	12'
0+555	12'	0+555	12'	0+555	12'
0+570	12'	0+570	12'	0+570	12'
0+585	12'	0+585	12'	0+585	12'
0+600	12'	0+600	12'	0+600	12'
0+615	12'	0+615	12'	0+615	12'
0+630	12'	0+630	12'	0+630	12'
0+645	12'	0+645	12'	0+645	12'
0+660	12'	0+660	12'	0+660	12'
0+675	12'	0+675	12'	0+675	12'
0+690	12'	0+690	12'	0+690	12'
0+705	12'	0+705	12'	0+705	12'
0+720	12'	0+720	12'	0+720	12'
0+735	12'	0+735	12'	0+735	12'
0+750	12'	0+750	12'	0+750	12'
0+765	12'	0+765	12'	0+765	12'
0+780	12'	0+780	12'	0+780	12'
0+795	12'	0+795	12'	0+795	12'
0+810	12'	0+810	12'	0+810	12'
0+825	12'	0+825	12'	0+825	12'
0+840	12'	0+840	12'	0+840	12'
0+855	12'	0+855	12'	0+855	12'
0+870	12'	0+870	12'	0+870	12'
0+885	12'	0+885	12'	0+885	12'
0+900	12'	0+900	12'	0+900	12'
0+915	12'	0+915	12'	0+915	12'
0+930	12'	0+930	12'	0+930	12'
0+945	12'	0+945	12'	0+945	12'
0+960	12'	0+960	12'	0+960	12'
0+975	12'	0+975	12'	0+975	12'
0+990	12'	0+990	12'	0+990	12'
0+1005	12'	0+1005	12'	0+1005	12'
0+1020	12'	0+1020	12'	0+1020	12'
0+1035	12'	0+1035	12'	0+1035	12'
0+1050	12'	0+1050	12'	0+1050	12'
0+1065	12'	0+1065	12'	0+1065	12'
0+1080	12'	0+1080	12'	0+1080	12'
0+1095	12'	0+1095	12'	0+1095	12'
0+1110	12'	0+1110	12'	0+1110	12'
0+1125	12'	0+1125	12'	0+1125	12'
0+1140	12'	0+1140	12'	0+1140	12'
0+1155	12'	0+1155	12'	0+1155	12'
0+1170	12'	0+1170	12'	0+1170	12'
0+1185	12'	0+1185	12'	0+1185	12'
0+1200	12'	0+1200	12'	0+1200	12'

NOTE: Use depressed curb at all driveways except as noted.

MISCELLANEOUS DETAILS