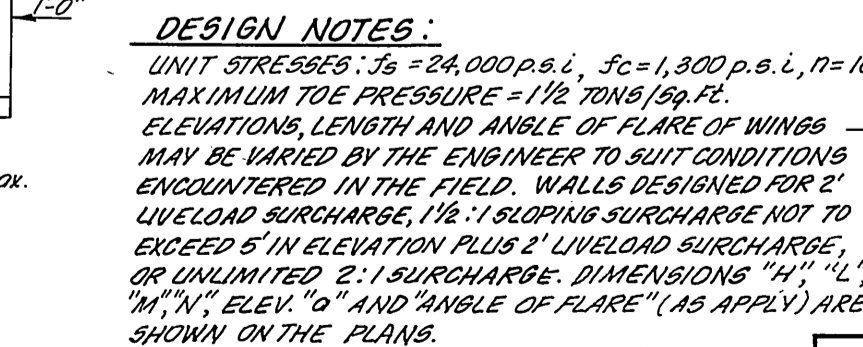
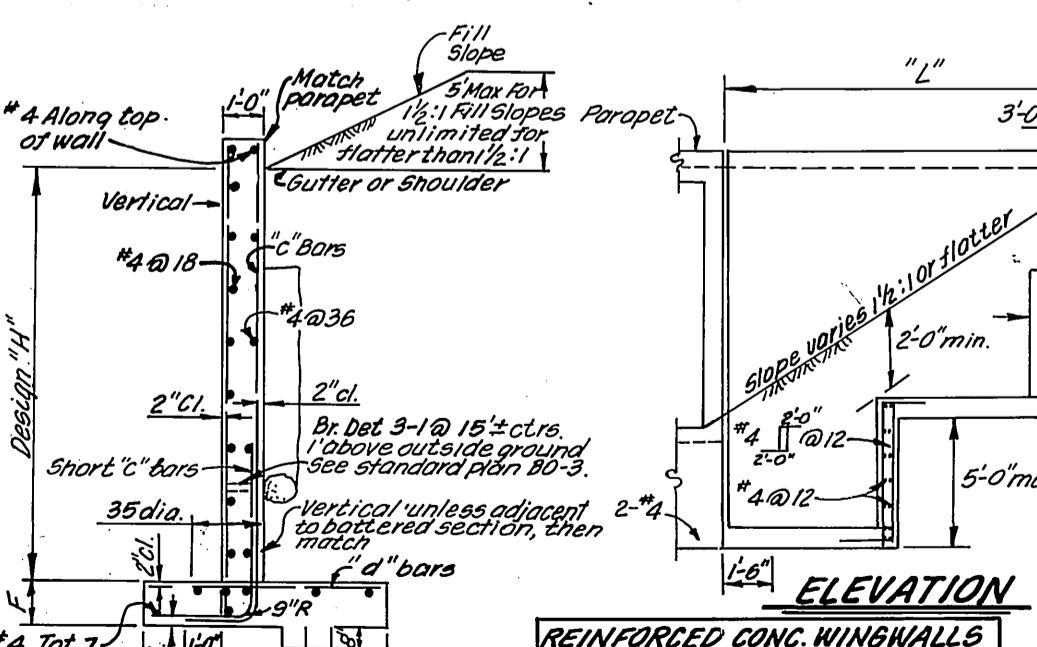


GENERAL NOTES

1. Construction shall be in accordance with the City of Riverside Department of Public Works, Standard Drawings, Standard Drawing No. 10 and the Standard Specifications for Public Works Construction, current edition.
2. It shall be the responsibility of the contractor to clear the right of way in accordance with the provisions of law as it affects each utility, including irrigation lines and appurtenances and at no cost to the city.
3. Trim edge of existing pavement where new pavement joins to a smooth, straight line. Construct curbs and gutters as shown per plan and overlay paving as directed in the field to provide a smooth surface.
4. Curb and gutter shall be staked with 3' offsets at 25' intervals.
5. All flagged elevations shall be staked in the field by the private engineer.
6. The private engineer signing these plans is responsible for assuring the accuracy and acceptability of the work hereon. In the event of discrepancies arising during construction as a result of errors or omissions, the private engineer shall be responsible for determining an acceptable solution and revising the plans for approval by the city.
7. The contractor shall be responsible for preserving or re-establishing and referring survey monuments destroyed, disturbed or buried as a result of construction shown hereon.
8. In pavement overlay areas, distressed existing pavement shall be removed and replaced as directed by the Inspector prior to overlaying with A.C. paving.
9. The contractor shall call in a location request to Underground Service Alert (USA), Phone No. 1-800-422-4133 two working days before digging. No construction permit will be issued by the Public Works Department involving excavation for underground facilities unless the applicant has been provided an inquiry identification number by U.S.A.
10. Any traffic control striping obliterated and/or badly worn during construction shall be restored by the contractor within five days after notification from the city inspectors.
11. Final striping for traffic control and/or pavement marking to be the responsibility of the developer.
12. All traffic signs shall be installed prior to opening the streets to traffic.

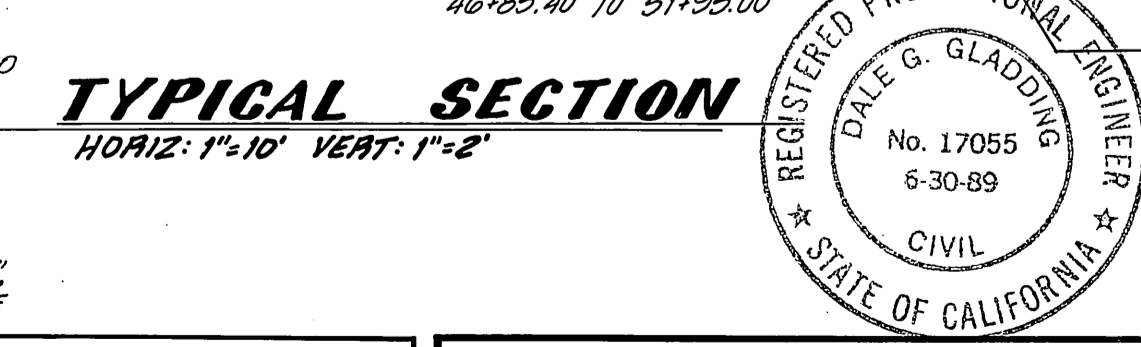


REINFORCED CONC. WINGWALLS

| W | H | A | B | C | D | E | F |
|-------------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|
| 14" | 4' | 5' | 7' | 9' | 11' | 13' | 15' |
| W | 9'-2" | 3'-0" | 4'-2" | 4'-8" | 5'-2" | 5'-8" | 6'-2" |
| H | 11'-0" | 12'-2" | 13'-4" | 14'-6" | 15'-8" | 17'-0" | 18'-2" |
| B | 2'-2" | 2'-6" | 2'-10" | 3'-2" | 3'-6" | 4'-0" | 4'-4" |
| F | 1'-2" | 1'-6" | 1'-10" | 2'-2" | 2'-6" | 3'-0" | 3'-4" |
| Catch Basin | 1'-0" | 1'-0" | 1'-0" | 1'-0" | 1'-0" | 1'-0" | 1'-0" |
| 1" Bars | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" |
| 2" Bars | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" |
| 3" Bars | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" |
| 4" Bars | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" |
| 5" Bars | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" |
| 6" Bars | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" |
| 7" Bars | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" |
| 8" Bars | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" |
| 9" Bars | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" |
| 10" Bars | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" |
| 11" Bars | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" |
| 12" Bars | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" |
| 13" Bars | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" |
| 14" Bars | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" |
| 15" Bars | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" |
| 16" Bars | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" |
| 17" Bars | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" |
| 18" Bars | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" |
| 19" Bars | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" |
| 20" Bars | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" |
| 21" Bars | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" |
| 22" Bars | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" |
| 23" Bars | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" |
| 24" Bars | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" |
| 25" Bars | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" |
| 26" Bars | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" |
| 27" Bars | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" |
| 28" Bars | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" |
| 29" Bars | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" |
| 30" Bars | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" | 18" @ 18" |

CONCRETE CURB AND GUTTER PER STR. DWG. #325
CONSTRUCT TYPE I CURB PER STR. DWG. #200, 5" C.F. 5' BUTTER
CONSTRUCT 4" AC OVER 12" CAB PER PLAN REF. R-2072-A

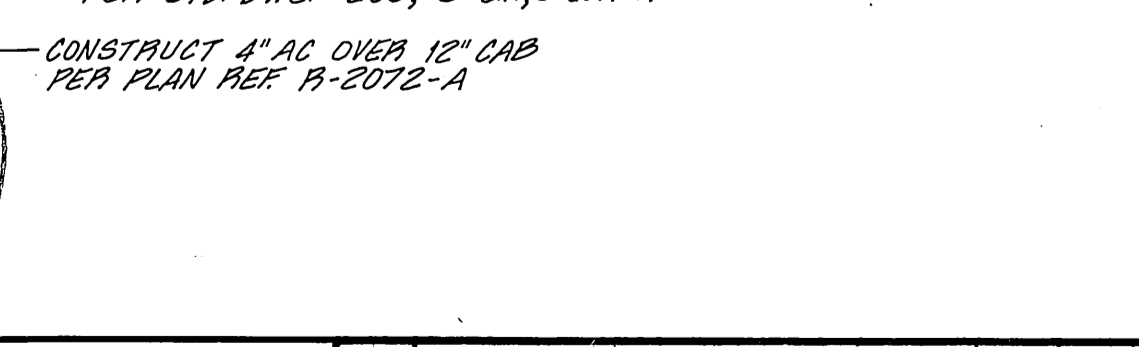
DESIGN NOTES:
UNIT STRESS: S_c = 24,000 p.s.i., S_c = 1,800 p.s.i., n = 10
MAXIMUM TOE PRESSURE = 1/2 TONS/100 LB. FE.
ELEVATIONS, LENGTH AND ANGLE OF FLARE OF WINGS MAY BE VARIED BY THE ENGINEER TO SUIT CONDITIONS ENCOUNTERED IN THE FIELD. WALLS DESIGNED FOR 2' UNIFORM SURCHARGE, 1/2" UNIFORM SURCHARGE NOT TO EXCEED 5' IN ELEVATION PLUS 1' UNIFORM SURCHARGE OR UNLIMITED SURCHARGE. DIMENSIONS "H", "L", "M", "N", "O" AND "ANGLE OF FLARE" (AS APPLY) ARE SHOWN ON THE PLANS.
WALL HEIGHT MAY BE EXCEEDED BY 6" BEFORE GOING TO NEXT GREATER "H". ELIMINATE CUT-OFF WALL IF ADJACENT CHANNEL IS PAVED AND SLOPE IS 20" MAXIMUM.



BENCHMARK:
PRECISE BM, H7-H
RM NAIL & TAG IN CENTER OF 6" CURB RETURN @ HANSAS & PENNSYLVANIA AVENUE.
ELEV. = 937.22

HAWKINS, ROBERTSON & ASSOCIATES
ENGINEERS - SURVEYORS
3764 ELIZABETH ST. RIVERSIDE, CALIFORNIA 92506
PH. (714) 684-9522

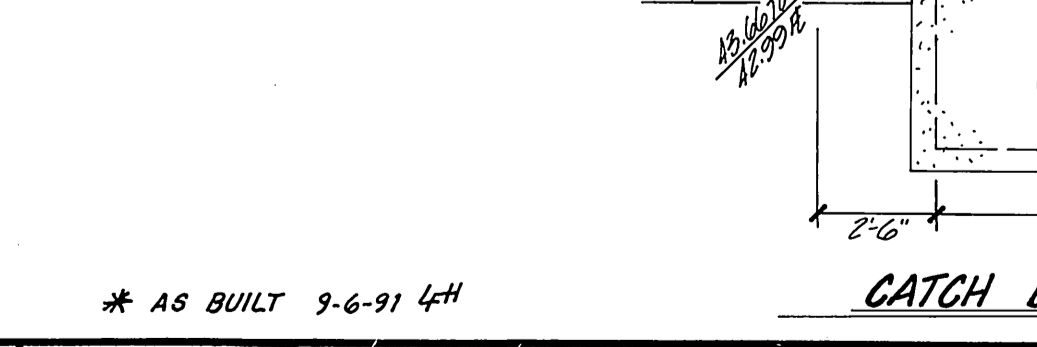
Dale S. Duddy 10-24-88
DALE G. GLADDING ACE 17055 DATE EXP 6-30-89



CITY OF RIVERSIDE
PUBLIC WORKS DEPARTMENT

APPROVED BY: Barry Paul
DATE: 6/29/89
PUBLIC WORKS DIRECTOR

PRINCIPAL ENGINEER: Barry Paul
DATE: 6/29/89
TRAFFIC DIVISION: Barry Paul
DATE: 6/29/89
CHIEF P.W. ENGINEER: Barry Paul
DATE: 6/29/89



STREET IMPROVEMENT PLAN
PENNSYLVANIA AVE.
AT BORDWELL PARK
BETWEEN HANSAS & OTTAWA
0-A-878

ACCOUNT NO. R-3045
SHEET 1 OF 2

HORIZ. SCALE: 1" = 40'
VERT. SCALE: 1" = 4'

City of Riv. Account No. 005193 Exp. 12-31-87

INDEXED 8-8-89 4H 3966

955 Lt

955 E

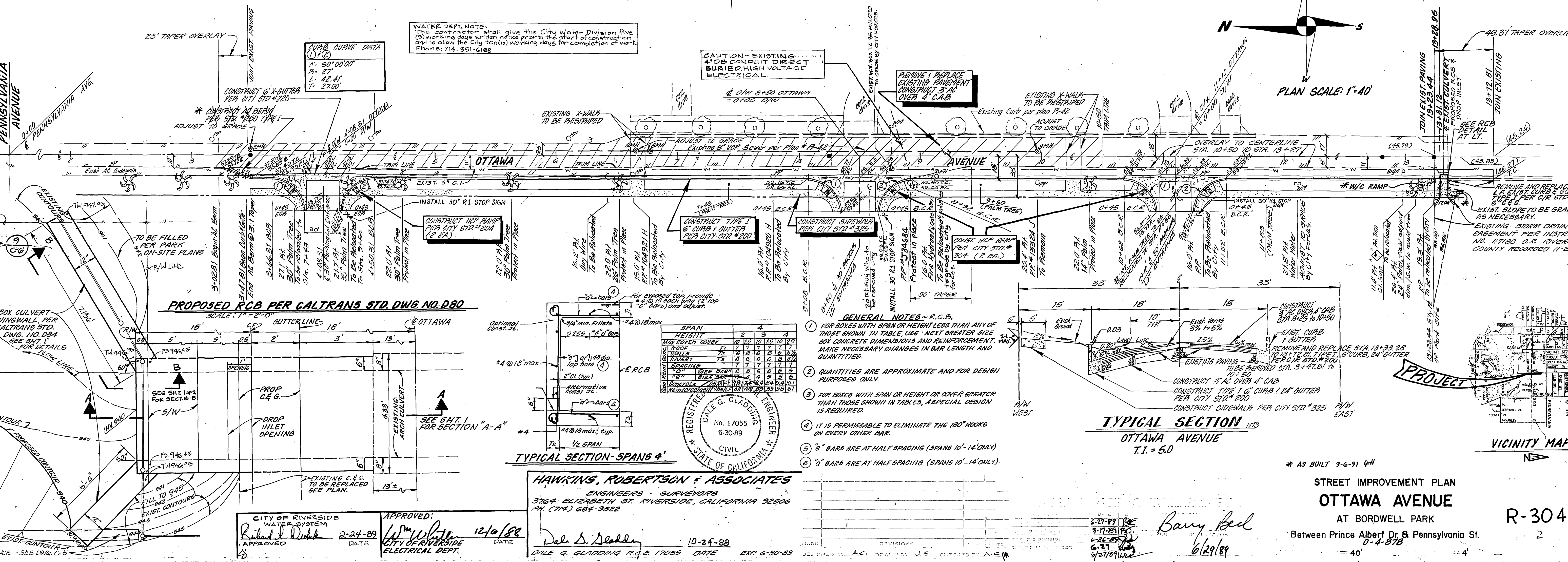
955 Rt

945 Lt

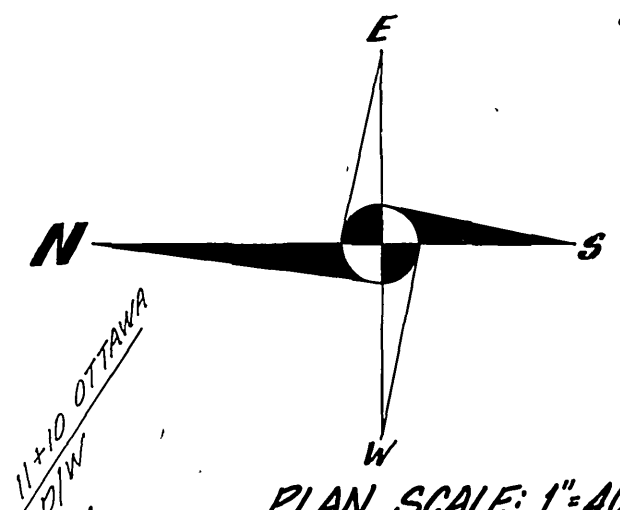
945 E

945 Pt

2 3 4 5 6 7 8 9 10 11 12 13 14



PROFILE SCALES:
 HORIZ. 1"=40'
 VERT. 1"=4'



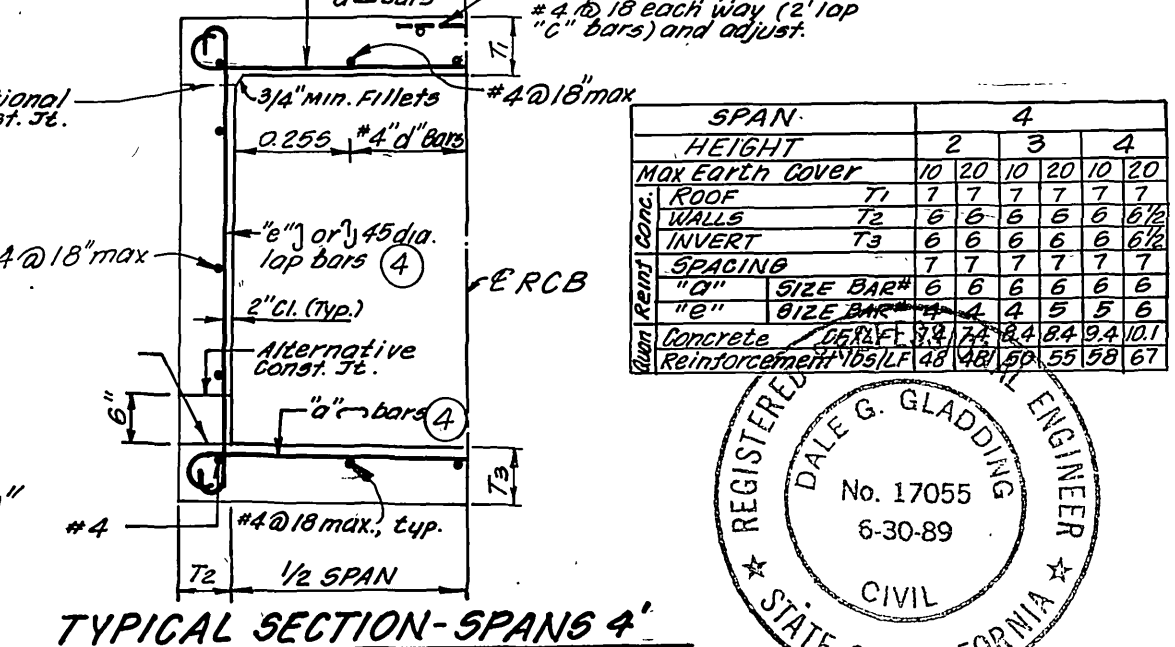
WATER DEPT. NOTE:
 The contractor shall give the City Water Division five (5) working days written notice prior to the start of construction and to allow the City ten (10) working days for completion of work. Phone: 714-351-6168

CAUTION-EXISTING 4" DB CONDUIT DIRECT BURIED HIGH VOLTAGE ELECTRICAL.

REMOVE & REPLACE EXISTING SIGNMENT CONDUIT 3" AC OVER 4" CAB.

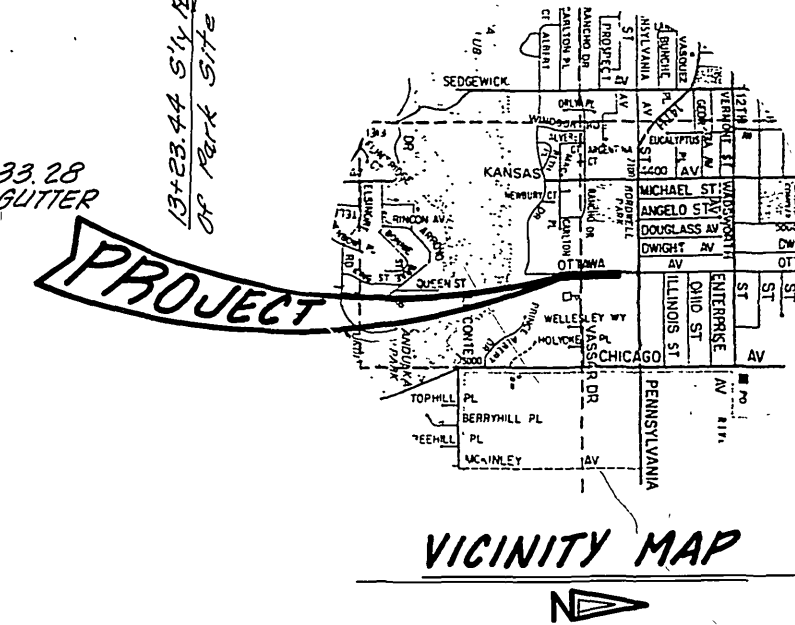
CURB CURVE DATA
 1. 90° 00' 00"
 2. 42.41'
 3. 27.00'

PROPOSED RCBS PER CALTRANS STD. DWG. NO. D80
 SCALE: 1"=2'-0"



- GENERAL NOTES - R.C.B.**
- FOR BOXES WITH SPAN OR HEIGHT LESS THAN ANY OF THOSE SHOWN IN TABLE, USE NEXT GREATER SIZE BOX CONCRETE DIMENSIONS AND REINFORCEMENT. MAKE NECESSARY CHANGES IN BAR LENGTH AND QUANTITIES.
 - QUANTITIES ARE APPROXIMATE AND FOR DESIGN PURPOSES ONLY.
 - FOR BOXES WITH SPAN OR HEIGHT OR COVER GREATER THAN THOSE SHOWN IN TABLES, SPECIAL DESIGN IS REQUIRED.
 - IT IS PERMISSIBLE TO ELIMINATE THE 180° HOOKS ON EVERY OTHER BAR.
 - 6" BARS ARE AT HALF SPACING (SPANS 10'-14' ONLY)
 - 8" BARS ARE AT HALF SPACING (SPANS 10'-14' ONLY)

TYPICAL SECTION
 OTTAWA AVENUE
 T.I. = 5.0



CITY OF RIVERSIDE WATER SYSTEM
 Richard D. Decker 2-24-89
 APPROVED: Dale S. Decker 12/6/88
 CITY OF RIVERSIDE ELECTRICAL DEPT. DATE

HAWKINS, ROBERTSON & ASSOCIATES
 ENGINEERS - SURVEYORS
 3764 ELIZABETH ST. RIVERSIDE, CALIFORNIA 92506
 PH. (714) 684-9522
 Dale S. Decker 10-24-88
 DATE G. GLADDING R.G.E. 17055 DATE EXP 6-30-89

STREET IMPROVEMENT PLAN
OTTAWA AVENUE
 AT BORDWELL PARK
 Between Prince Albert Dr. & Pennsylvania St.
 0-4-87B

R-3045