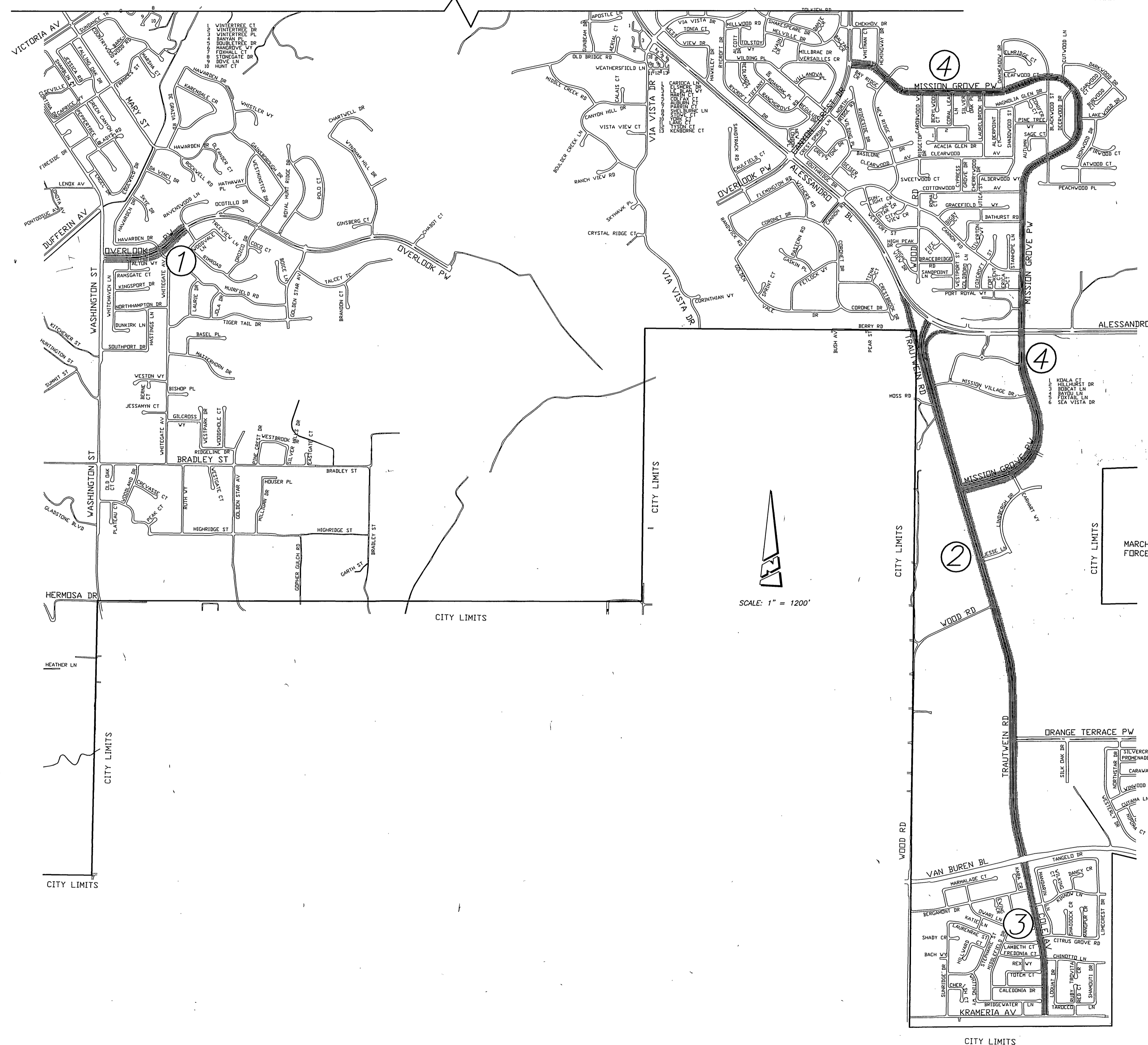
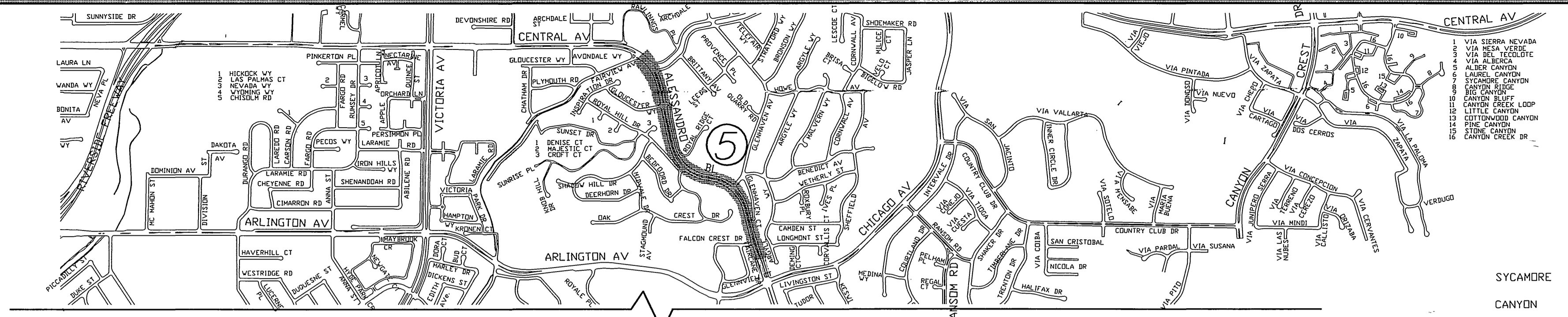


ARTERIAL STREET REHABILITATION LOCATIONS

NO.	STREET NAME	STREET LENGTH (L.F.)	EMULSION AGGREGATE SLURRY (S.Y.)	CURB GUE FOR WHEELCHAIR RAMPS (L.F.)	CRACK FILL (L.F.)	R&R C&G (L.F.)
1	OVERLOOK PARKWAY - WASHINGTON STREET TO RAVENSWOOD LANE	1,800	13,440	24	-	96
2	TRAUTWEIN ROAD - ALESSANDRO BOULEVARD TO VAN BUREN BOULEVARD	12,200	88,720	80	100	100
3	COLE AVENUE - VAN BUREN BOULEVARD TO KRAMERIA AVENUE	3,000	20,000	30	-	96
4	MISSION GROVE PARKWAY - CANYON CREST DRIVE TO TRAUTWEIN ROAD	13,700	77,570	228	100	468
5	ALESSANDRO BLVD. - FAIRVIEW AVENUE TO 300' N'LY OF ARLINGTON AVENUE/CHICAGO AVENUE	3,400	24,440	24	200	85
TOTAL		34,100	224,170	386	400	845

GENERAL NOTES:

- CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF RIVERSIDE DEPARTMENT OF PUBLIC WORKS STANDARD DRAWINGS, AND THE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION, CURRENT EDITION AND THE SPECIAL PROVISIONS.
- THE CONTRACTOR IS REQUIRED TO TAKE DUE PRECAUTIONARY MEASURES TO LOCATE AND PROTECT ALL UTILITIES WITHIN THE CONSTRUCTION WORK AREAS.
- ANY TEMPORARY TRAFFIC CONTROL STRIPING OBLITERATED AND/OR BADLY WORN DURING CONSTRUCTION SHALL BE RESTORED BY THE CONTRACTOR WITHIN ONE DAY AFTER NOTIFICATION FROM THE CITY INSPECTOR.
- IN PAVEMENT OVERLAY AREAS, ANY DISTRESSED EXISTING PAVEMENT SHALL BE REMOVED AND REPLACED AS DIRECTED BY THE INSPECTOR PRIOR TO OVERLAYING WITH ARHM-GG PAVING.
- STREET IMPROVEMENT WORK THAT WILL CAUSE DAMAGE TO THE DETECTOR LOOP(S) SHALL NOT BE PERFORMED UNTIL TRAFFIC SIGNAL MAINTENANCE PERSONNEL HAVE BEEN NOTIFIED AND THE NECESSARY TIMING ADJUSTMENTS ARE MADE TO MAINTAIN SIGNAL OPERATION.
- FINAL STRIPING FOR TRAFFIC CONTROL AND/OR PAVEMENT MARKING TO BE THE RESPONSIBILITY OF THE CITY.
- ALL CRACKS IN THE PAVEMENT FROM 1/2" TO 1" IN WIDTH SHALL BE FILLED WITH CRAFCO BRAND "POLYFLEX TYPE III"; "MODIFIED ASPHALT RUBBER SEALANT" OR CITY APPROVED EQUAL. APPLICATION SHALL BE WITH A DOUBLE BOILER HEAT SYSTEM SUCH AS A CRAFCO EZ POUR 200 OR CITY APPROVED EQUAL AND TO MANUFACTURER'S RECOMMENDATIONS. ALL CRACKS GREATER THAN 1" IN WIDTH SHALL BE FILLED WITH F-AR-4000 A.C. (SEE SPECIAL PROVISIONS).
- MANHOLES AND VALVE COVERS WITHIN THE COLD MILLING OR PAVEMENT REPLACEMENT LIMITS SHALL NOT BE ADJUSTED TO GRADE. INSTEAD, COLD MILLING OR PAVEMENT REMOVAL BY OTHER MEANS SHALL BE DONE TO PROVIDE A SMOOTH FINISHED SURFACE TRANSITION AROUND THE COMPLETE CIRCUMFERENCE OF THE STRUCTURE. EXCEPTIONS TO THIS ARE WHERE MANHOLES AND VALVE COVERS ARE WITHIN PAVEMENT REMOVAL AREAS BUT ARE TOO CLOSE (WITHIN 3') TO THE ADJACENT PROPOSED A.C. OVERLAY.
- THE INSPECTOR MAY REQUEST COLD MILLING AND PAVING INTO SIDE STREET TO IMPROVE THE RIDE QUALITY AND OTHER STREET CONDITIONS.
- THE PLANS DO NOT TYPICALLY INDICATE LOCATIONS OF WATER VALVES OR FIRE HYDRANTS, HOWEVER, THESE FACILITIES MUST BE LOCATED DURING CONSTRUCTION TO MAKE VALVE BOX AND COVER ADJUSTMENTS AND BLUE HYDRANT MARKER INSTALLATIONS. THE VALVE BOXES MUST BE "TIED OUT" AS PART OF THE CONTRACTOR'S WORK.
- PAVEMENT PREPARATION SHALL CONSIST OF REMOVAL AND REPLACEMENT (R&R) OF DETERIORATED PAVEMENT AREAS AS MARKED BY THE INSPECTOR AT THE TIME OF CONSTRUCTION. QUANTITIES SHOWN ARE BASED UPON R&R SECTIONS BEING 0.50' IN DEPTH.
- THE SEAM BETWEEN PASSES OF THE A.C. OVERLAY SHALL COINCIDE WITH THE LANE LINES, UNLESS OTHERWISE PERMITTED BY THE INSPECTOR FOR EXTENUATING CIRCUMSTANCES.



ENGINEER IN RESPONSIBLE CHARGE
Michael Katusian
 MICHAEL KATUSIAN
 R.C.E. No. 36686 expires 6/30/00
 DATE 7/2/99

REGISTERED PROFESSIONAL ENGINEER
 MICHAEL KATUSIAN
 No. 36686
 Exp. 6-30-00
 CIVIL
 STATE OF CALIFORNIA

CITY OF RIVERSIDE, CALIFORNIA
 PUBLIC WORKS DEPARTMENT

APPROVED BY: *R. Mc. Smith* PUBLIC WORKS DIRECTOR
 DATE: 7-8-99

DESIGNED BY: CS DRAWN BY: LPH CHECKED BY:

REHABILITATION OF
 ARTERIAL STREETS AT
 VARIOUS LOCATIONS
 1998 - 1999
 (SLURRY SEAL)

ACCT. NO. 430-541500-440222-31030
R-3499
 SHEET: 1 OF 2
 FILE NO.

INDEXED 9-7-99 411

CONSTRUCTION LEGEND:

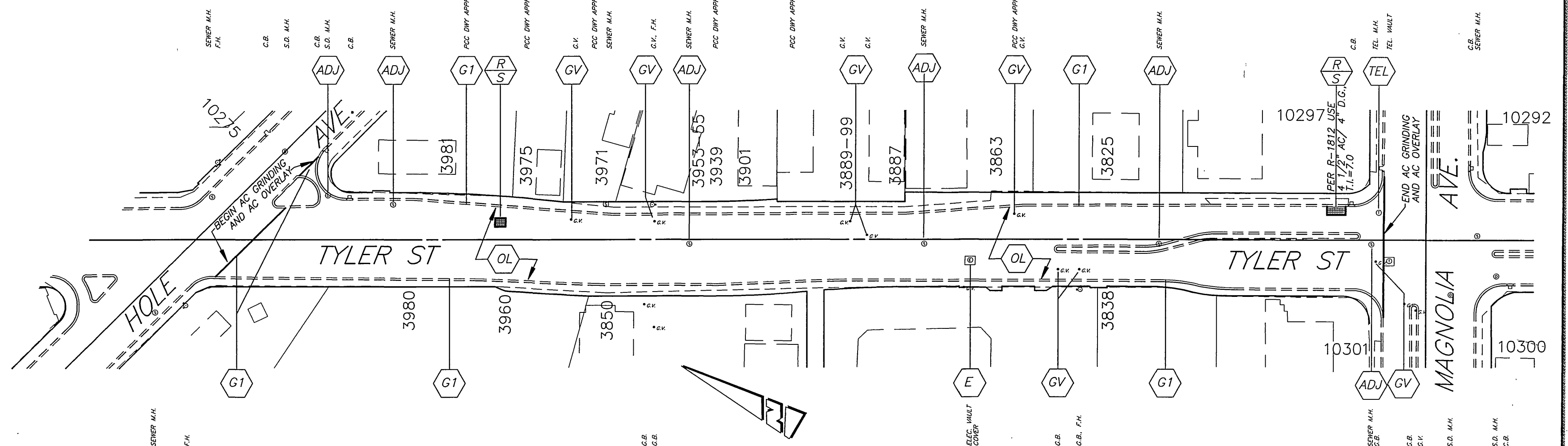
- (R/S)** REMOVE & REPLACE DETERIORATED PAVEMENT AREAS WITH FULL DEPTH AC AT THE DIRECTION OF THE ENGINEER.
- (G1)** WHERE THE PAVEMENT OVERLAY IS ADJACENT TO THE GUTTER LIP, COLD MILL THE EXISTING PAVEMENT TO THE DEPTH OF 0.08' BELOW THE GUTTLER LIP AND DAYLIGHT TO MEET THE EXISTING PAVEMENT IN 5'. WHERE THE PAVEMENT OVERLAY IS TO MEET THE EXISTING PAVEMENT AT THE JOIN LINE COLD MILL THE EXISTING PAVEMENT TO A DEPTH OF .08' BELOW THE JOIN LINE AND DAYLIGHT TO MEET THE EXISTING PAVEMENT IN A DISTANCE OF 10' FROM THE JOIN LINE.
- (OL)** OVERLAY EXISTING AC SURFACE WITH 0.10' ARHM-GG.
- (ADJ)** ADJUST SEWER MANHOLE, STORM DRAIN MANHOLE AND/OR WATER MANHOLE COVER TO GRADE.
- (GV)** ADJUST WATER GATE VALVE COVER TO GRADE.
- (E)** ADJUST ELECTRICAL MANHOLE COVER TO GRADE.
- (TEL)** TELEPHONE MANHOLE COVER TO BE ADJUSTED TO GRADE BY THE TELEPHONE COMPANY.
- (R/GV)** REPLACE WATER VALVE BOX AND COVER.
- (TDL 2)** REPLACE EXISTING TRAFFIC DETECTOR LOOPS. NUMBER OF DETECTOR LOOPS.
- (TDL PIP)** TRAFFIC DETECTOR LOOPS. PROTECT IN PLACE.

CALTRANS GENERAL NOTES:

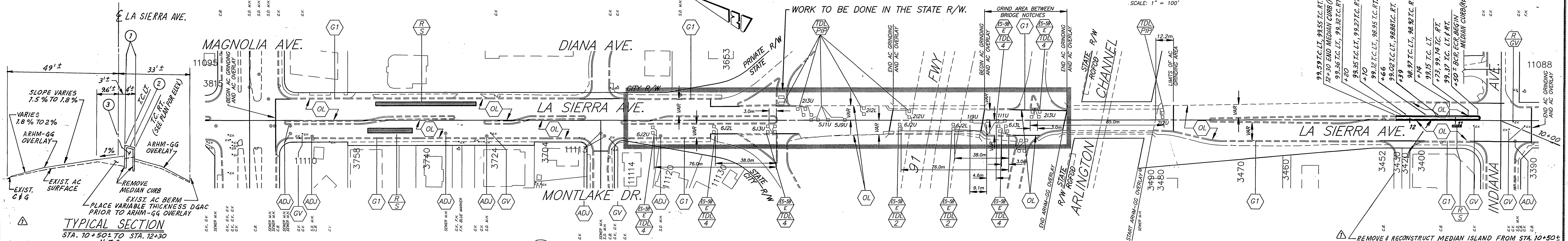
1. THE CONTRACTOR SHALL APPLY FOR AND OBTAIN AN ENCROACHMENT PERMIT FROM CALTRANS, BEFORE BEGINNING ANY WORK WITHIN THE STATE RIGHT-OF-WAY.
2. ALL WORK WITHIN THE STATE RIGHT-OF-WAY, SHALL CONFORM TO THE LATEST STATE STANDARD PLANS AND SPECIFICATIONS OR AS DIRECTED BY THE STATE'S REPRESENTATIVE. STANDARDS OTHER THAN STATE STANDARDS MUST BE PRE-APPROVED AND JUSTIFIED.
3. NO EQUIPMENT OR MATERIALS MAY BE STORED IN THE STATE RIGHT-OF-WAY.
4. ALL DISTURBED AREAS IN THE STATE RIGHT-OF-WAY MUST BE TREATED FOR EROSION CONTROL (HYDRO-SEEDING OR EQUIVALENT OR AS DIRECTED BY THE STATE'S REPRESENTATIVE). THE RESPONSIBILITY FOR MAINTAINING EROSION CONTROL WILL NOT BE RELEASED UNTIL THE SEEDING IS WELL ESTABLISHED. THE CONTRACTOR WILL BE RESPONSIBLE FOR CATRANS COST OF CLEANING DRAINAGE STRUCTURES OR CHANNELS CLUTTERED WITH DEBRIS AND/OR SILT AS A RESULT OF, OR CAUSED BY THE CONSTRUCTION PROJECT.
5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING THAT ANY STATE DRAINAGE FACILITY, WHICH IS CONNECTED TO OR DIRECTLY AFFECTED BY THE CONTRACTOR'S OPERATION, SHALL BE OPERATIONAL PRIOR TO FINAL ACCEPTANCE OF THE PERMIT WORK BY THE STATE. ADEQUATE CLEANOUTS AND ACCESS OPENINGS SHALL BE PROVIDED IN ANY CONSTRUCTION WITHIN THE STATE'S RIGHT-OF-WAY FOR FUTURE MAINTENANCE AND REPAIR WORK AS NEEDED. THIS WORK SHALL BE FURNISHED AT NO COST TO THE STATE.
6. WHERE SURVEY MONUMENTS EXIST, THEY SHALL BE PROTECTED OR SHALL BE REFERENCED AND RESET PURSUANT TO BUSINESS AND PROFESSIONS CODE, SECTIONS 8700 TO 8805 (LAND SURVEYOR'S ACT).
7. WHERE REQUIRED, THE PAVEMENT MUST BE SAW-CUT AT THE LANE LINE OR A MINIMUM 2 FEET FROM THE EXISTING EDGE OF PAVEMENT THE SAW-CUTS MUST BE PERPENDICULAR TO, PARALLEL OR CONCENTRIC WITH THE STATE HIGHWAY CENTERLINE.
8. ALL SIGNS, ROADSIDE MARKERS, ELECTROLIERS ETC., SHALL BE PROTECTED AND/OR REPLACED IN-KIND, AT NO COST TO THE STATE, IN ACCORDANCE WITH THE CURRENT STATE STANDARD PLANS AND THE LATEST EDITION OF THE TRAFFIC MANUAL.
9. NO FREEWAY RAMP OR FREEWAY LANES SHALL BE CLOSED OR OBSTRUCTED AT ANY TIME UNLESS SPECIFICALLY ALLOWED PER THE ENCROACHMENT PERMIT AND/OR AS DIRECTED BY THE STATE'S REPRESENTATIVE.
10. ALL FENCING, RELOCATED TO FACILITATE THE CONSTRUCTION OF THIS PROJECT INSIDE THE STATE RIGHT-OF-WAY, SHALL BE REPLACED WITH TYPE C1-6 FENCING AS SHOWN IN THE STATE'S STANDARD PLANS.
11. ALL SIGNING, STRIPING AND PAVEMENT MARKINGS SHALL BE IN CONFORMANCE WITH THE CURRENT EDITION OF THE TRAFFIC MANUAL, PUBLISHED BY THE STATE OF CALIFORNIA, DEPARTMENT OF TRANSPORTATION, AND THE SPECIAL PROVISIONS. ALL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE NOTED ON THE PLANS.
12. ALL CONFLICTING STRIPING AND PAVEMENT MARKINGS, NOT SHOWN ON THE PLANS, SHALL BE REMOVED FROM THE PAVEMENT BY SANDBLASTING BY THE CONTRACTOR.
13. ANY FACILITY DAMAGED BY CONTRACTOR OPERATIONS SHALL BE REPAIRED OR REPLACED, AT CONTRACTOR'S EXPENSE PROMPTLY, IN ACCORDANCE WITH STATE SPECIFICATIONS.

ARTERIAL STREET REHABILITATION LOCATIONS

NO.	STREET NAME	STREET LENGTH (L.F.)	CRACK FILL (L.F.)	CRACK REPAIR (L.F.)	0.1' ARHM-GG OVERLAY (TON)	COLD MILLING TYPE 1 (5' WIDE) (L.F.)	COLD MILLING TYPE 1 (10' WIDE) (L.F.)	CURB GRINDING FOR WHEELCHAIR RAMPS (L.F.)	TRAFFIC SIGNAL DETECTOR LOOP (EA.)	R&R AC PAVEMENT (C.F.)	R&R C&G (L.F.)	ADJUST SEWER, STORM DRAIN &/OR WATER M.H. COVER TO GRADE (EA.)	ADJUST G.V. COVER TO GRADE (EA.)	ADJUST ELECTRIC M.H. COVER TO GRADE (EA.)	ADJUST SEWER CLEANOUT COVER TO GRADE (EA.)
1	TYLER STREET - MAGNOLIA AVENUE TO HOLE AVENUE	1,320	400	200	900	2,740	270	-	14	218	-	6	9	1	-
2	LA SIERRA AVENUE - MAGNOLIA AVENUE TO INDIANA AVENUE	2,360	6,350	300	1,480	3,460	430	17	83	300	50	9	6	-	-
3	INDIANA AVENUE - MARY STREET TO ARLINGTON AVENUE	2,400	2,000	500	1,220	4,390	440	23	44	1,398	75	11	23	2	2
	TOTAL	6,080	8,750	1,000	3,600	10,590	1,140	40	141	1916	125	26	38	3	2



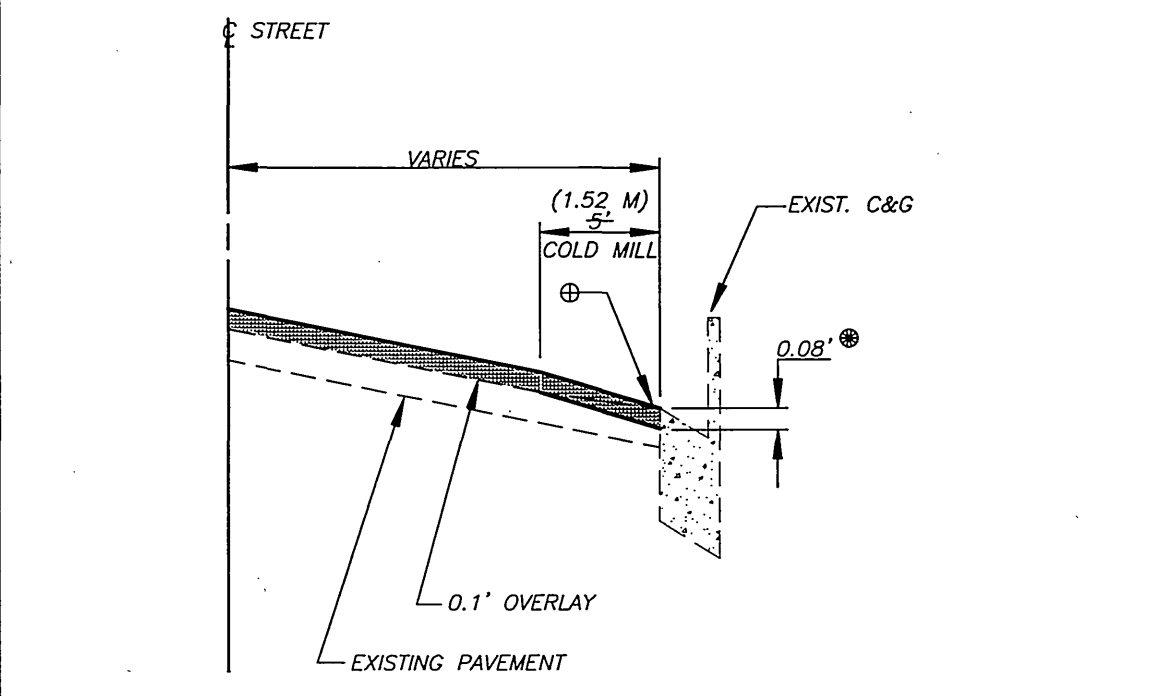
1 TYLER STREET - MAGNOLIA AVENUE TO HOLE AVENUE



2 LA SIERRA AVENUE - MAGNOLIA AVENUE TO INDIANA AVENUE

CONST. NOTES:

1. CONSTRUCT PCC CURB TYPE II 6" C.F.
2. CONSTRUCT STAMPED CONCRETE AT MEDIAN ISLAND. COLOR, TILE PATTERN, AND FINISH SHALL MATCH EXISTING STAMPED CONCRETE.
3. CONSTRUCT COMPACTED SANDY FILL MATERIAL AT MEDIAN ISLAND.

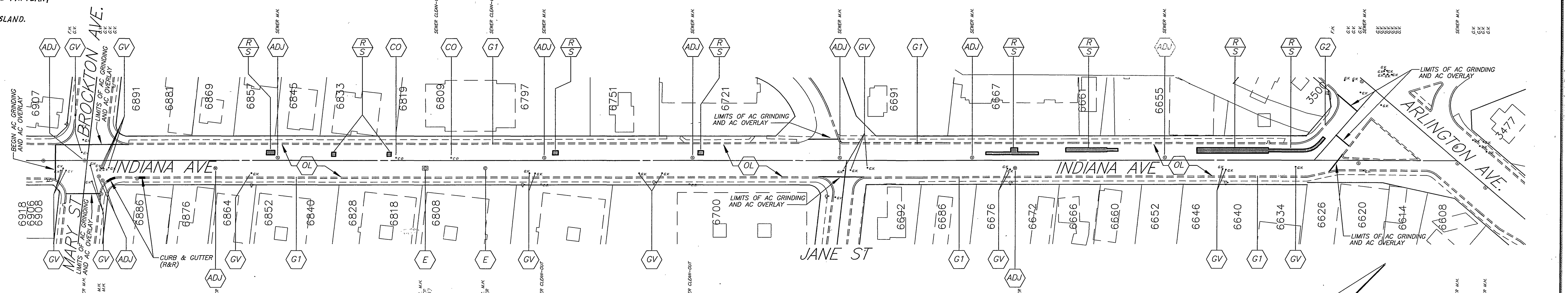


TYPICAL SECTION

PAVING & COLD MILLING DETAIL

- COLD MILLING DEPTH AT GUTTER LIP MAY BE DECREASED WHERE MACADAM BASE IS ENCOUNTERED.
- ⊕ ASPHALT RUBBER HOT MIX SHALL BE ROLLED TO WITHIN 1/4" TO FLUSH WITH THE GUTTER LIP.

PAVING & COLD MILLING DETAIL



3 INDIANA AVENUE - MARY STREET TO ARLINGTON AVENUE

<p>ENGINEER IN RESPONSIBLE CHARGE Michael Katuzian MICHAEL KATUZIAN R.C.E. No. 36686 expires 6/30/00 DATE 7/3/99</p>	<p>Remove & Reconstruct Median Island</p>	<p>CITY OF RIVERSIDE, CALIFORNIA PUBLIC WORKS DEPARTMENT</p>	<p>REHABILITATION OF ARTERIAL STREETS AT VARIOUS LOCATIONS 1998 - 1999 (ARHM-GG OVERLAY)</p>	<p>ACCT. NO. 430-541500-440222-31030 R-3499 SHEET 2 OF 2 FILE NO.</p>
	<p>APPROVED BY: R. M. Math PRINCIPAL ENGINEER STREET SERVICES SUBDIVISION TRAFFIC DIVISION INSPECTION ENGR. MANAGER</p>	<p>APPROVED BY: R. M. Math PUBLIC WORKS DIRECTOR</p>	<p>DATE: 7-8-99</p>	<p>DESIGNED BY: CS DRAWN BY: LPH CHECKED BY:</p>

INDEXED 9-7-99 G/H