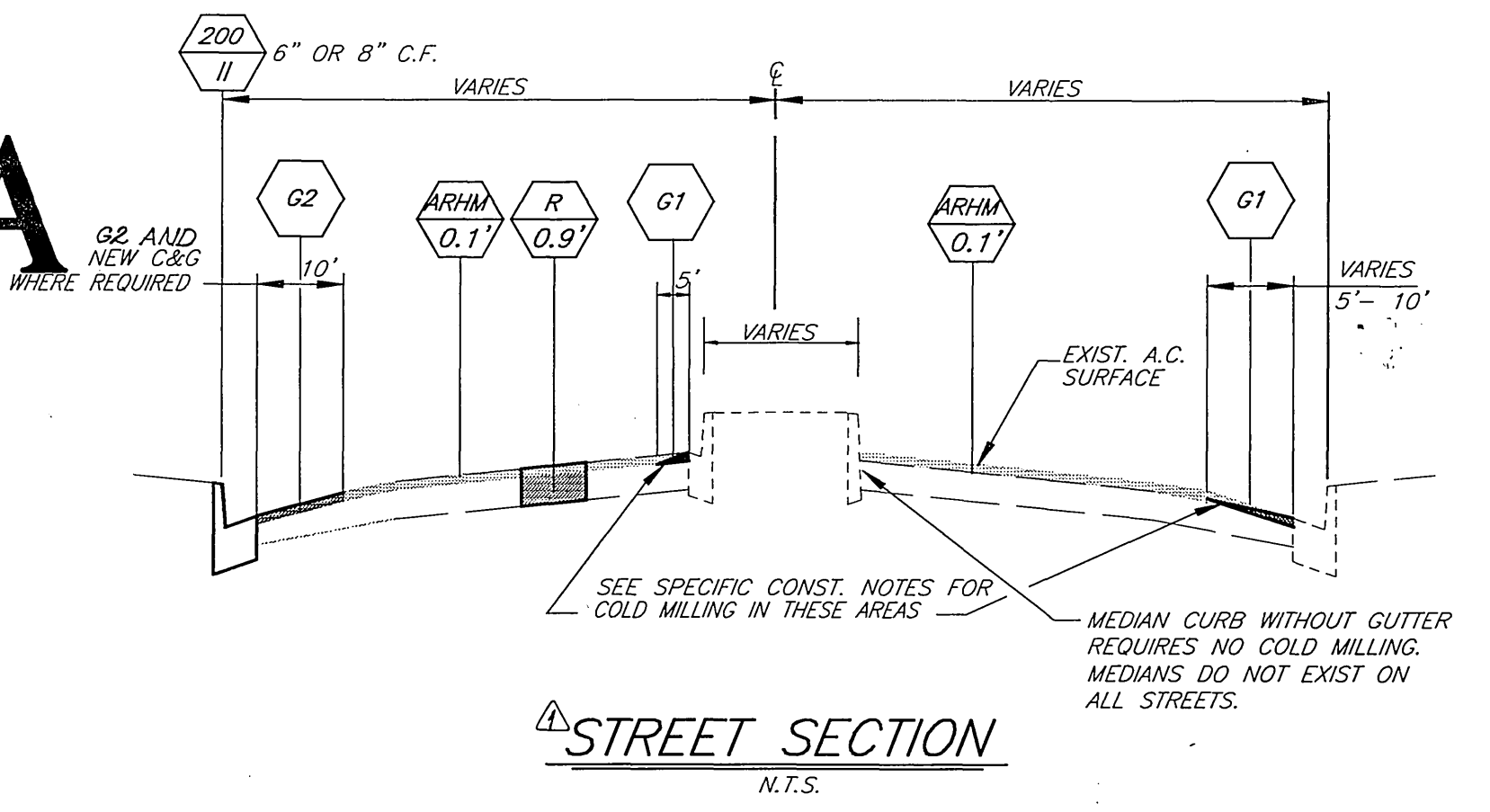


RIVERSIDE CALIFORNIA

PUBLIC WORKS DEPARTMENT

REHABILITATION OF ARTERIAL STREETS 1998-1999

STPL- 5058 (054)



GENERAL CONSTRUCTION NOTES

- 1 CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF RIVERSIDE STANDARD DRAWINGS AND STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION 1997 EDITION.
- 2 PRIME CONTRACTORS BIDDING THIS WORK SHALL HAVE A CLASS A OR C-12 LICENSE.
- 3 THE SEAM BETWEEN PASSES OF THE AC OVERLAY SHALL COINCIDE, AS CLOSE AS POSSIBLE, WITH THE LANE LINES.
- 4 GRADE SHEETS SHALL BE REQUIRED FOR CURB, C&G OR EP REMOVALS GREATER THAN 50' IN LENGTH. THE CONTRACTOR SHALL ALLOW 96 HRS AFTER REQUESTING SURVEY FOR SCHEDULING SURVEY AND PROCESSING GRADE SHEETS FOR REPLACEMENT OF THE IMPROVEMENTS. DEPENDING ON THE ACTUAL QUANTITY OF C&G SELECTED ON MAIN ST., IT MAY BE NECESSARY TO ALLOW ADDITIONAL TIME FOR EXTRA SURVEY & DESIGN WORK REQUIRED.
- 5 ALL CRACKS IN PAVEMENT 1/2" TO 1" IN WIDTH SHALL BE FILLED WITH A RUBBERIZED ASPHALT MATERIAL AND ALL CRACKS GREATER THAN 1" IN WIDTH SHALL BE FILLED WITH F-AR-4000 A.C. PRIOR TO APPLICATION OF ARHM - GG (SEE SPECIAL PROVISIONS).
- 6 MANHOLES AND VALVES WITHIN THE GRINDING OR PAVEMENT REPLACEMENT LIMITS SHALL NOT BE ADJUSTED TO GRADE. INSTEAD, ENOUGH GRINDING (OR PMVT. REMOVAL BY OTHER MEANS) SHALL BE DONE TO PROVIDE A SMOOTH FINISHED SURFACE TRANSITION AROUND THE COMPLETE CIRCUMFERENCE OF THE STRUCTURE. EXCEPTIONS TO THIS ARE WHERE MHS AND VALVES ARE WITHIN PMVT. REMOVAL AREAS BUT ARE TOO CLOSE (WITHIN 3') TO THE ADJACENT PROPOSED A.C. OVERLAY.
- 7 PULL BOXES FOR SPlicing TRAFFIC LOOP DETECTOR CONDUCTORS AND LEAD-IN CABLE ARE NORMALLY ADJACENT TO DETECTOR LOOPS.
- 8 EXACT RESURFACING LIMITS SHALL BE MARKED IN THE FIELD PRIOR TO CONSTRUCTION. THE INSPECTOR MAY REQUEST GRINDING, PAVING INTO SIDE STREETS AND EXTEND R&R OF DETERIORATED PAVEMENT TO IMPROVE RIDE QUALITY AND OTHER STREET CONDITIONS.
- 9 MANHOLES, WATER VALVES AND FIRE HYDRANTS MUST BE LOCATED DURING CONSTRUCTION TO MAKE MANHOLE AND VALVE BOX ADJUSTMENTS AND BLUE HYDRANT MARKER INSTALLATION. THE VALVE BOXES MUST BE "TIED OUT" AS PART OF THE CONTRACTORS WORK.
- 10 PLAN INDICATES ONLY THE QUANTITY OF PAVEMENT REMOVAL FOR EACH STREET. ACTUAL REMOVAL AND REPLACEMENT LIMITS OF DETERIORATED PAVEMENT AREAS WILL BE MARKED BY THE INSPECTOR PRIOR TO THE TIME OF CONSTRUCTION. PAVEMENT R&R QUANTITIES ARE SHOWN IN THE PLAN TABLE FOR BID ESTIMATING PURPOSES ONLY AND ACTUAL QUANTITY WILL VARY. THESE QUANTITIES WERE ESTIMATED BY FIELD OBSERVATIONS TO BE SUFFICIENTLY ACCURATE FOR BID PURPOSES.
- 11 ALL IDENTIFIED LOOPS MUST BE REPLACED.

STREET	LIMITS	TRAFFIC INDEX	LENGTH
1 CHICAGO AVENUE	LE CONTE TO UNIVERSITY	9.5	4950' 0.94 MILE
2 INDIANA AVENUE	MADISON TO MARY	9.0	4000' 0.76 MILE
3 LINCOLN AVENUE	JEFFERSON TO MADISON	8.0	2720' 0.52 MILE
4 MADISON STREET	INDIANA TO MAGNOLIA	9.0	3800' 0.72 MILE
5 MAIN STREET	THIRD TO 60 FREEWAY	10.0	3800' 0.72 MILE
6 MARKET STREET	60 FREEWAY TO CITY LIMITS	10.0	2450' 0.46 MILE
TOTAL			21,720' 4.12 MILE 17,920 3.40

SPECIFIC CONSTRUCTION NOTES

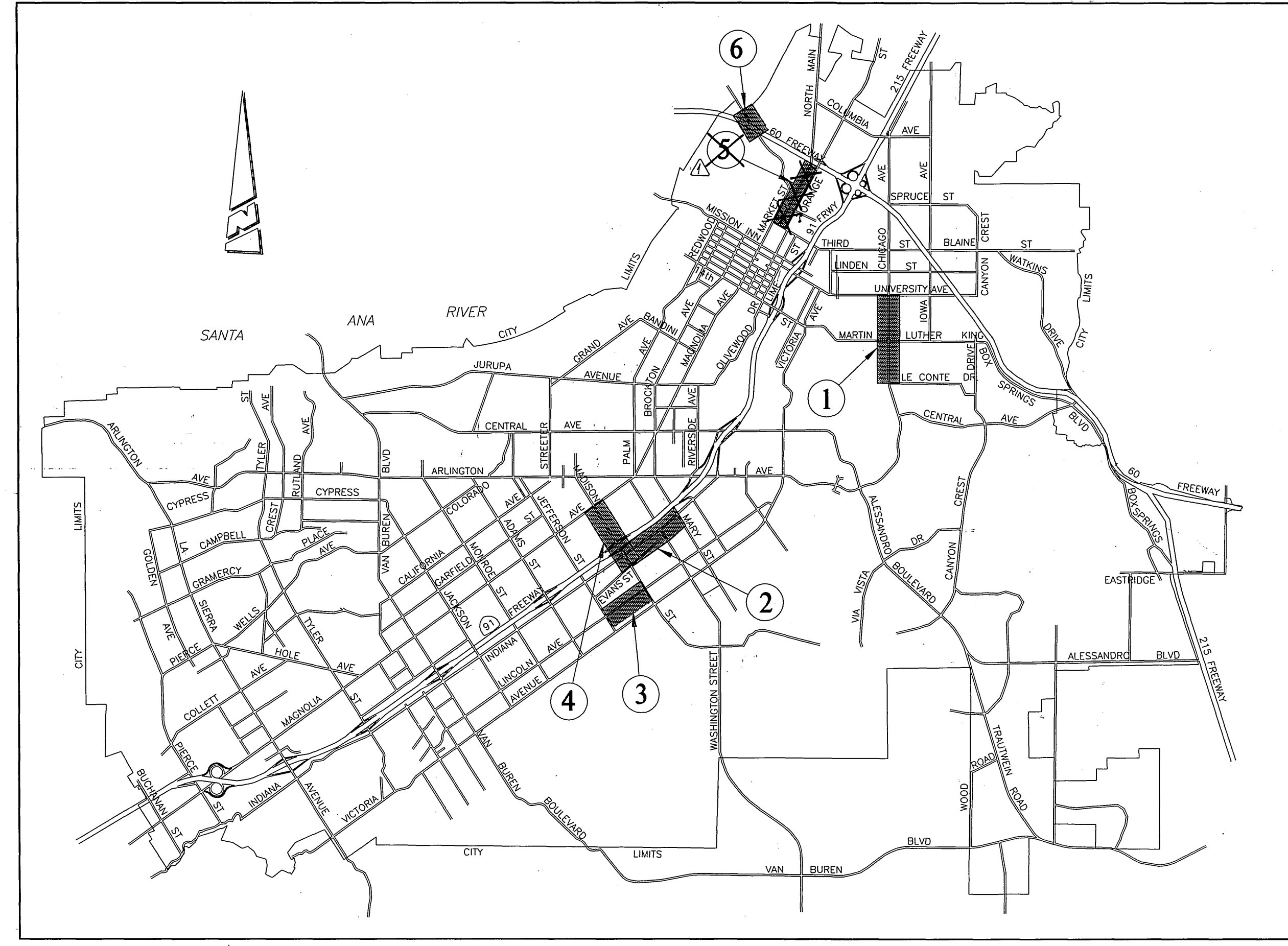
- NUMBERS ABOVE 100 IN A HEXAGON ON THE PLAN REFER TO THE RESPECTIVE STANDARD DRAWING AND SHALL BE CONSTRUCTED ACCORDINGLY. UNLESS MODIFICATIONS ARE NOTED ON THE PLAN OR COVER SHEET, ANY NUMBER SHOWN BELOW THE STANDARD DRAWING NUMBER INDICATES THE SPECIFIC ALTERNATE TO BE CONSTRUCTED.
- ARHM 0.17 CONSTRUCT ASPHALT RUBBER HOT MIX GAP GRADED (ARHM-GG) OVERLAY. OVERLAY THICKNESS
- G1 COLD MILL EXIST. AC 0.08' BELOW GUTTER LIP OR OUTER EDGE, "DAY-LIGHTING" IN 5' (TYPE 1 GRINDING). THE SURFACE OF AREAS WITH 10' WIDE COLD MILL, SHOWN ON THE PLAN, SHALL BE STRAIGHT GRADED IN ONE PASS FROM 0.1' AT THE OUTSIDE EDGE TO "DAY LIGHT" AT INSIDE EDGE.
- G2 COLD MILL EXIST. AC 0.2' BELOW EXIST. AC SURFACE OR ADJACENT P.C.C. CUTTER (TYPE 2 GRINDING) STRAIGHT GRADE THE 10' WIDE COLD MILLED STRIP TO "DAY LIGHT" AT THE EXIST. A.C. SURFACE IN ONE PASS.
- R 0.9 REMOVE EXISTING PAVEMENT TO SAME DEPTH OF PAVEMENT STRUCTURAL SECTION. CONSTRUCT FULL DEPTH AC STRUCTURAL SECTION AS LISTED IN TABLE BELOW.

PAVEMENT STRUCTURAL SECTION

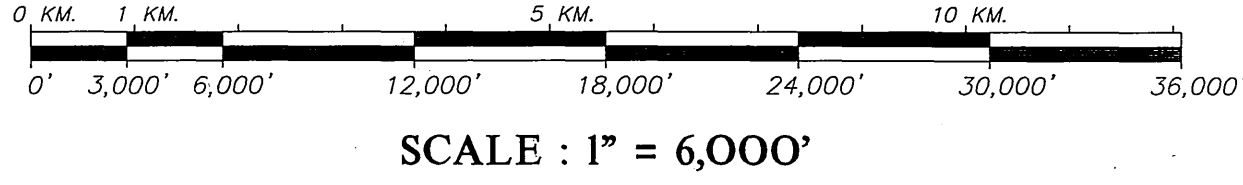
STREET	R-VALUE	THICK. FT.
CHICAGO AVENUE	50	0.68'
INDIANA AVENUE	45	0.73'
LINCOLN AVENUE	55	0.54'
MADISON STREET	30	0.90'
MARKET STREET	64	0.63'

CALTRANS GENERAL NOTES

1. The contractor shall apply for and obtain an encroachment permit from Caltrans, before beginning any work within State right of way.
2. All work within the State right-of-way, shall conform to the latest State Standard Plans and Specifications or as directed by the State's Representative. Standards other than State Standards must be pre-approved and justified.
3. No equipment or materials may be stored in the State right of way.
4. All disturbed areas in the State right-of-way must be treated for erosion control (hydro-seeding or equivalent or as directed by the State's Representative). The responsibility for maintaining erosion control will not be released until the seeding is well established. The contractor will be responsible for Caltrans's cost of cleaning any drainage structures or channels cluttered with debris and/or silt as a result of, or caused by the construction project.
5. The contractor shall be responsible for ensuring that any State drainage facility, which is connected to or directly affected by the contractor's operation, shall be operational prior to final acceptance of the permit work by the State. Adequate cleanouts and access openings shall be provided in any construction within the State's right-of-way for future maintenance and repair work as needed. This work shall be furnished at no cost to the State.
6. Where survey monuments exist, they shall be protected or shall be referenced and reset pursuant to Business and Professions Code, Sections 8700 to 8805 (Land Surveyor's Act).
7. Where required, the pavement must be saw-cut at the lane line or a minimum 2 feet from the existing edge of the pavement. The saw-cuts must be perpendicular to, parallel or concentric with the State Highway centerline.
8. All signs, roadside markers, electrolites, etc., shall be protected and/or replaced in-kind, at no cost to the State, in accordance with the current State Standard Plans and the latest edition of the Traffic Manual.
9. No freeway ramps or freeway lanes shall be closed or obstructed at any time unless specifically allowed per the encroachment permit and/or as directed by the State's Representative.
10. All fencing, relocated to facilitate the construction of this project inside the State right-of-way, shall be replaced with type CL-6 fencing as shown in the State's Standard Plans.
11. All signing, striping and pavement markings shall be in conformance with the current edition of the Traffic Manual, published by the State of California, Department of Transportation, and the Special Provisions. All pavement markings shall be thermoplastic unless otherwise noted on the plans.
12. All conflicting striping and pavement markings, not shown on the plans, shall be removed from the pavement by sandblasting by the contractor.
13. Any facility damaged by contractor operations shall be repaired or replaced, at contractor's expense promptly, in accordance with State specifications.

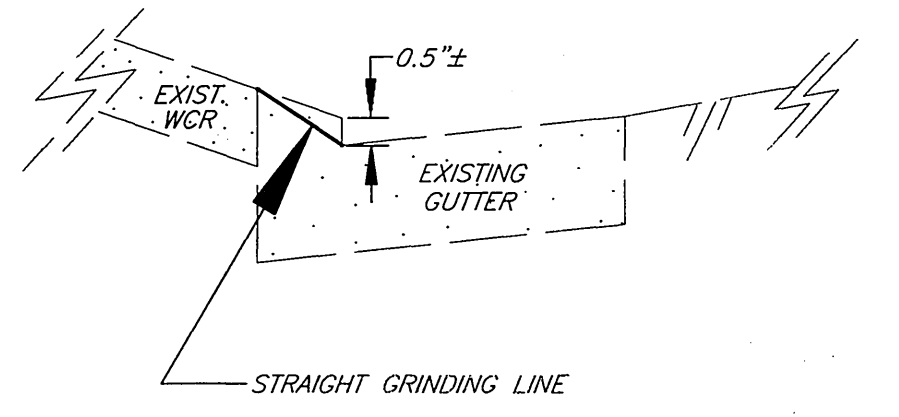


PROJECT LOCATION MAP



REHABILITATION QUANTITIES BY STREET LOCATION

No.	ITEM	UNIT	CHICAGO	INDIANA	LINCOLN	MADISON	MAIN	MARKET	TOTAL
1	AC Pavement Removal	CY	135	260	220	270	420	495	1,380
2	AC Pavement Replacement (DGAC)	TON	270	520	450	550	845	1,000	2,790
3	Asphalt Rubber Hot Mix-Gap Graded	TON	2,860	2,160	1,240	2,180	1,880	1,375	9,815
4	R&R PCC C&G, Std. 200, Type I (6")	LF	100	300	140	235	1,864	100	875
5	R&R PCC C&G, Std. 200, Type I (8")	LF	40	30	40	40	80	30	180
6	R&R PCC Sidewalk, Std. 325	SF	150	300	740	2,780	4,000	400	4,340
7	R&R PCC Cross Gutters, Std. 220	SF	100	40	40	200	200	1,140	1,520
8	R&R PCC Driveway Approach, Std. 302	SF	200	300	200	200	1,300	200	1,100
9	Cold Milling-Type I (5'Wide)	LF	9,200	6,840	3,850	6,400	6,500	3,900	30,190
10	Cold Milling-Type I (10'Wide)	LF	1,600	670	1,600	1,200	1,000	1,100	6,170
11	Cold Milling-Type II (10'Wide)	LF	600	0	0	0	700	0	600
12	Crack Fill (F)	LF	4,950	4,000	2,720	3,800	2,800	2,450	17,920
13	Crack Repair	LF	1,980	1,600	1,080	1,520	1,420	1,000	7,180
14	Adjust Sewer or SD MH. to Grade	EA.	20	12	14	16	16	4	66
15	Replace Water Valve Box and Cover	EA.	4	4	4	4	4	4	20
16	Adjust Water Valve Frame and Cover to Grade	EA.	35	37	35	35	35	35	177
17	Adjust Water Manhole to Grade	EA.	2	2	2	2	2	2	10
18	Adjust Electric Vault to Grade	EA.	1	1	1	1	1	1	5
19	Detector Loop Installation	EA.	34	29	24	60	70	0	147
20	Type I - Two Way Blue Reflective Marker	EA.	10	10	10	10	10	10	50
21	Grind Existing Curb Depression at WCR	LF	75	75	67	145	50	30	392
22	Cold Milling-Type III	SF							78,000
									78,000



WCR CURB FACE GRINDING DETAIL
(NTS)

IMPORTANT NOTICE
Section 4216/4217 of the Government Code requires a Dig Alert Identification Number to be issued before a Permit to Excavate will be valid. For your Dig Alert ID Number call CALL TOLL FREE TWO WORKING DAYS BEFORE YOU DIG UNDERGROUND SERVICE ALERT 1-800-422-4133

ENGINEER IN RESPONSIBLE CHARGE
Ronald C. Mueller
R.C.E. No. 29510 expires 3/31/03
DATE 7-27-99

REGISTERED PROFESSIONAL ENGINEER
RONALD C. MUELLER
No. 29510
Exp. 3-31-03
CIVIL
STATE OF CALIFORNIA

CITY OF RIVERSIDE, CALIFORNIA PUBLIC WORKS DEPARTMENT
APPROVED BY: [Signature]
DATE: 7-27-99

REHABILITATION OF ARTERIAL STREETS 1998-1999 STPL- 5058 (054)

ACCT. NO. 430-541500-440125-30230
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SHEET 1 OF 1
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