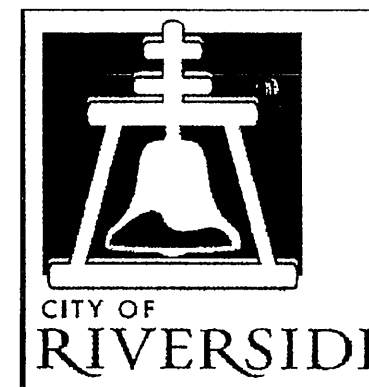


RIVERSIDE

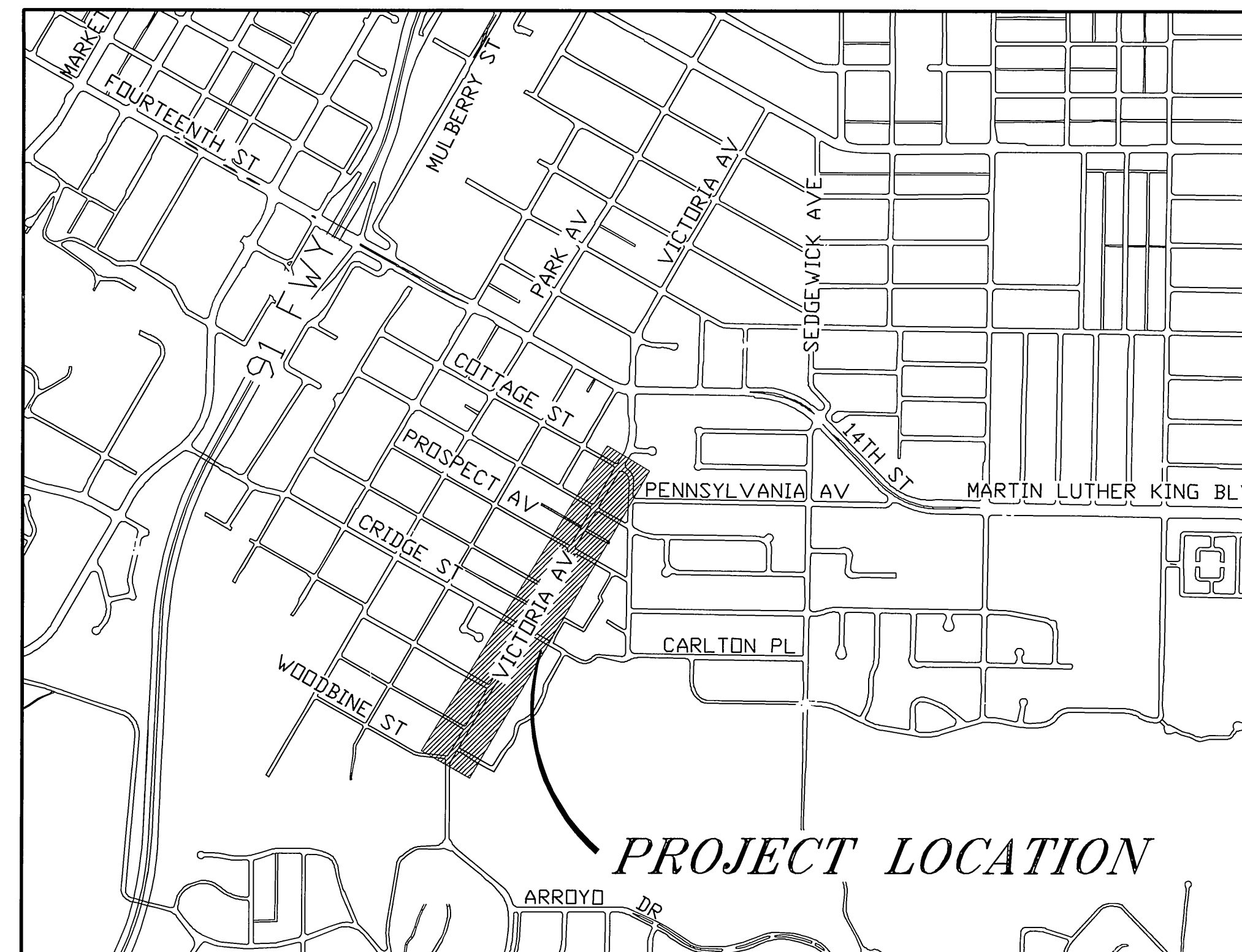


CALIFORNIA

PUBLIC WORKS DEPARTMENT VICTORIA AVENUE REHABILITATION FROM COTTAGE STREET TO WOODBINE STREET

GENERAL NOTES:

- PRIME CONTRACTOR BIDDING THIS PROJECT SHALL HAVE A CLASS "A" OR CLASS "C-12" LICENSE.
- CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF RIVERSIDE DEPARTMENT OF PUBLIC WORKS, STANDARD DRAWINGS AND THE "STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION", 2006 EDITION.
- THE CONTRACTOR IS REQUIRED TO TAKE DUE PRECAUTIONARY MEASURES TO LOCATE AND PROTECT ALL UTILITIES WITHIN THE CONSTRUCTION WORK AREAS. MH'S & WATER VALVES SHALL BE "TIED OUT" PRIOR TO PAVEMENT OVERLAY WORK FOR SUBSEQUENT ADJUSTMENT WORK.
- ANY TEMPORARY TRAFFIC CONTROL STRIPING OBLITERATED AND/OR BADLY WORN DURING CONSTRUCTION SHALL BE RESTORED BY THE CONTRACTOR WITHIN ONE DAY AFTER NOTIFICATION FROM THE CITY INSPECTOR.
- IN PAVEMENT OVERLAY AREAS, ANY DISTRESSED EXISTING PAVEMENT SHALL BE REMOVED AND REPLACED AS DIRECTED BY THE INSPECTOR AFTER COLD-MILLING AND PRIOR TO OVERLAYING WITH ARHM-GG PAVING.
- WHERE NEW PAVEMENT IS TO JOIN THE EXISTING PAVEMENT, THE EDGE OF THE EXISTING PAVEMENT SHALL BE TRIMMED IN A SMOOTH AND STRAIGHT LINE. CONSTRUCT MATCH-UP PAVING AS DIRECTED IN THE FIELD TO PRODUCE A SMOOTH SECTION.
- STREET IMPROVEMENT WORK THAT WILL CAUSE DAMAGE TO THE DETECTOR LOOP'S SHALL NOT BE PERFORMED UNTIL TRAFFIC SIGNAL MAINTENANCE PERSONNEL HAVE BEEN NOTIFIED AND THE NECESSARY TIMING ADJUSTMENTS ARE MADE TO MAINTAIN SIGNAL OPERATION.
- THE CONTRACTOR IS RESPONSIBLE FOR THE FINAL STRIPING AND PAVEMENT MARKING. STRIPING AND PAVING WORK SHALL BE COMPLETED PER PLANS, CITY STANDARDS AND SPECIAL PROVISIONS.
- SURVEY STAKING REQUESTS AND GRADE SHEETS ARE REQUIRED FOR THE REPLACEMENT OF ALL CURBS AND/OR GUTTER OVER 50' IN LENGTH. SURVEY REQUESTS SHALL BE SUBMITTED AFTER THE EXISTING IMPROVEMENTS HAVE BEEN REMOVED. CONTRACTOR SHALL ALLOW 4 WORKING DAYS AFTER REQUESTING SURVEY TO SCHEDULE SURVEY AND PROCESS GRADE SHEETS. CONTRACTOR SHALL PROVIDE TRAFFIC CONTROL FOR ALL REQUIRED CONSTRUCTION SURVEY WORK BY CITY PERSONEL. NO STAKING WILL BE REQUIRED FOR SIDEWALK, DWY APPROACHES AND WHEELCHAIR RAMP CONSTRUCTION.
- CONTRACTOR MAY USE SINGLE/DOUBLE ADJUSTMENT OR RAMP-UP OPTIONS FOR MANHOLES AND VALVE COVERS WITHIN THE COLD MILLING AND/OR PAVEMENT REPLACEMENT LIMITS. RAMP-UP OPTION SHALL HAVE SMOOTH FINISH AND SAFE TRANSITION AROUND THE CIRCUMFERENCE OF THE STRUCTURE. RAMP-UP WORK SHALL BE ACCEPTED BY THE INSPECTOR BEFORE ROAD IS OPENED TO TRAFFIC. PAVING MATERIALS USED FOR RAMP-UP SHALL BE REMOVED COMPLETELY BEFORE OVERLAY/PAVING OPERATION TAKES PLACE.
- THE INSPECTOR MAY DIRECT COLD-MILLING AND PAVING INTO SIDE STREETS TO IMPROVE THE RIDE QUALITY AND OTHER STREET CONDITIONS.
- WATER VALVES AND FIRE HYDRANTS MUST BE LOCATED DURING CONSTRUCTION TO MAKE VALVE BOX AND COVER ADJUSTMENTS AND FIRE HYDRANT BLUE MARKER INSTALLATIONS. THE VALVE BOXES MUST BE "TIED OUT" AS PART OF THE CONTRACTOR'S WORK.
- PAVEMENT PREPARATION SHALL CONSIST OF REMOVAL AND REPLACEMENT (R&R) OF DETERIORATED PAVEMENT AREAS AS MARKED IN THE FIELD AND DIRECTED BY THE INSPECTOR AT THE TIME OF CONSTRUCTION. QUANTITIES SHOWN ARE APPROXIMATE AND BASED UPON ALL PREMARKED REMOVAL AND REPLACEMENT SECTIONS AS INDICATED ON THESE PLANS. PAVEMENT REMOVAL AND REPLACEMENT SHALL BE DONE AFTER COLD MILLING IS COMPLETED.
- THE SEAM BETWEEN PASSES OF THE AC OVERLAY SHALL COINCIDE, AS CLOSE AS POSSIBLE, TO THE LANE LINES.
- UNIFORM COLD MILLING TYPE II SHALL BE AS DIRECTED BY THE ENGINEER AND AS DESCRIBED IN SPECIAL PROVISIONS.
- THIS PROJECT HAS APPROXIMATELY 12 NEW RECONSTRUCTED WHEELCHAIR RAMPS. MOST RAMPS REQUIRE THE REMOVAL AND REPLACEMENT OF APPROXIMATELY 100 TO 150 SF OF RAMP AREA, 50 TO 100 SF OF SIDEWALK, 30 LF OF CURB AND GUTTER, 30 LF OF NEW RETAINING CURB. SOME RAMPS MAY REQUIRE APPROXIMATELY 200 SF OF ASPHALT CONCRETE PAVEMENT REMOVAL AND REPLACEMENT FOR CROSS-FALL. THESE QUANTITIES ARE APPROXIMATE AND MAY CHANGE ACCORDING TO EACH RAMP TYPE AND SITE CONDITIONS. RAMPS SHALL BE CONSTRUCTED PER CITY STANDARDS, ADA REQUIREMENTS AND AS DIRECTED BY THE ENGINEER. AREAS FOR REMOVAL SHALL BE MARKED BY THE CONTRACTOR AND ACCEPTED BY THE CITY ENGINEER AT THE TIME OF CONSTRUCTION. FOR LIST OF RAMP LOCATIONS AND TYPES, SEE SPECIAL PROVISIONS.



LOCATION MAP
N.T.S.

QUANTITIES TABLE

#	STREET NAME	STREET LIMIT	STREET LIMIT	STREET LENGTH (LF)	STREET AREA (SF)	AC REMOVAL (SF)	AC REPLACE (TON)	R&R DWY APP (SF)	AC OVERLAY (TON)	WCR (EA)	R&R CURB & GUTTER (LF)	R&R SIDEWALK (SF)	COLD MILLING TYPE 2 UNIFORM WIDTH	ADJ SEWER & S.D. (EA)	VALVES
1	Victoria Avenue	Cottage Street	Woodbine Street	2,700	115,000	30,000	500	650	1,600	12	100	1,600	115,000	6	19

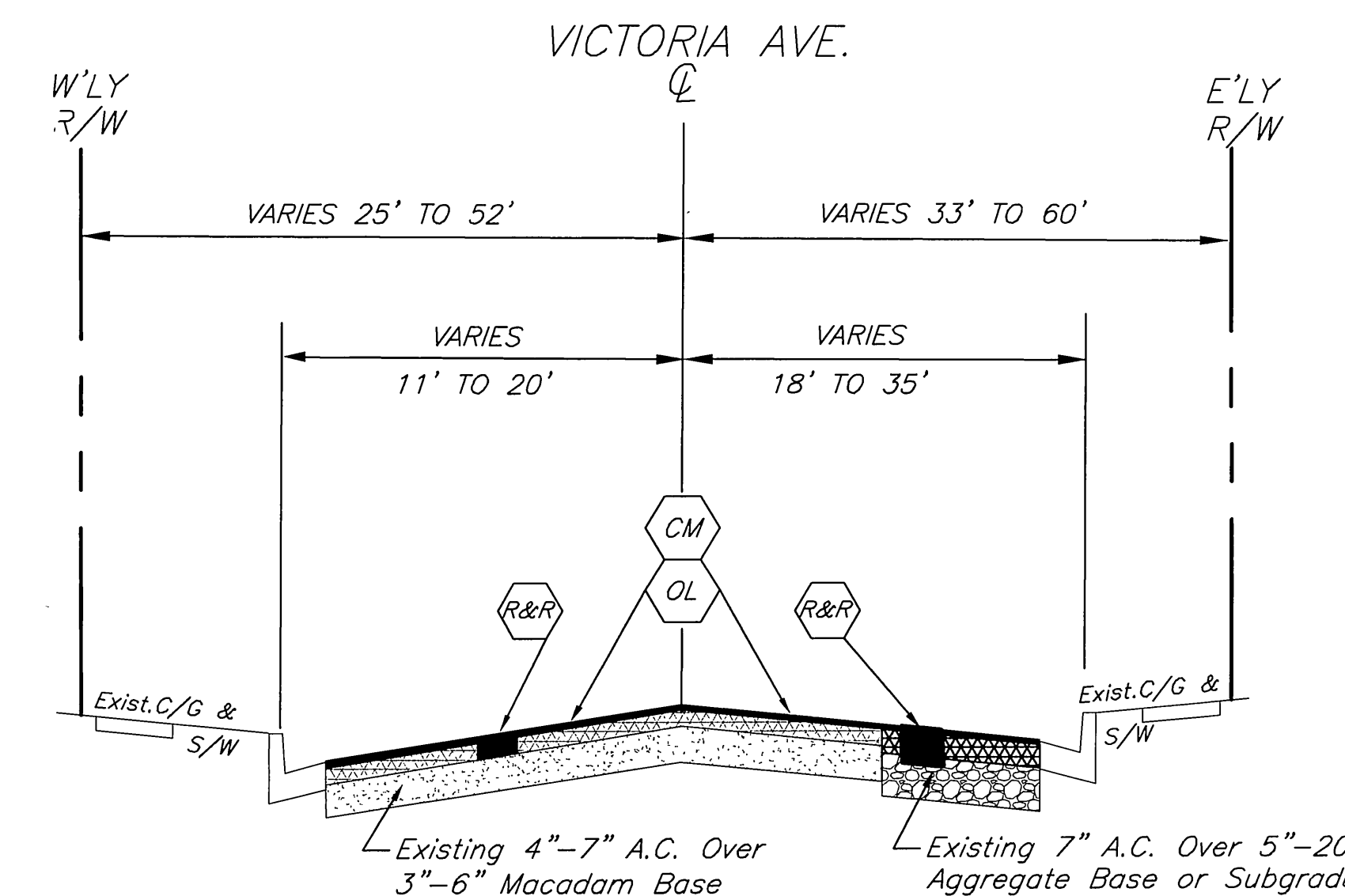
* The entire AC pavement removal and replacement quantities shown on the plan may not be used. AC removal and replacement areas shall be marked after cold-milling operations are completed and AC pavement inspected by the inspector.

CONSTRUCTION NOTES:

- NUMBERS ABOVE 100 IN A HEXAGON ON THE PLAN REFER TO THE RESPECTIVE STANDARD DRAWING AND SHALL BE CONSTRUCTED ACCORDINGLY. UNLESS MODIFICATIONS ARE NOTED ON THE PLAN OR COVER SHEET. ANY NUMBER SHOWN BELOW THE STANDARD DRAWING NUMBER INDICATES THE SPECIFIC ALTERNATE TO BE CONSTRUCTED.
- OVERLAY THE ENTIRE PAVEMENT WIDTH ON VICTORIA AVENUE WITH 2" THICK A.R.H.M.-GG. OVERLAY SHALL EXTEND TO CURB RETURNS OF ALL CROSS STREETS ALONG VICTORIA AVENUE.
- BASED ON AS-BUILT PLANS AND CORING INFORMATION FOR VICTORIA AVENUE BETWEEN COTTAGE STREET AND WOODBINE STREET, IT HAS BEEN ASSUMED THAT THE EXISTING ASPHALT PAVEMENT STRUCTURAL SECTIONS FOR THE ANTICIPATED AC REMOVAL AND REPLACEMENT AREAS GENERALLY INCLUDE THE FOLLOWING: 4"-7" AC OVER MACADAM BASE (APPROXIMATELY 26,000 SF) OR 7" AC OVER 5"-20" AGGREGATE BASE OR SUBGRADE (APPROXIMATELY 4,000 SF). THE ACTUAL AREAS FOR REMOVAL AND REPLACEMENT OF DETERIORATED PAVEMENT WILL BE MARKED IN THE FIELD BY THE INSPECTOR AFTER THE 2"-UNIFORM COLD-MILLING OPERATION IS COMPLETED. IF THE ENGINEER DETERMINES THAT THE REMAINING DEPTH OF DETERIORATED PAVEMENT AREAS WHERE 4"-7" OF AC OVER MACADAM BASE EXISTS, CONTRACTOR WILL BE REQUIRED TO COLD-MILL ADDITIONAL 2" OF EXISTING AC PAVEMENT (TO A TOTAL DEPTH OF 4" OR UNTIL MACADAM IS EXPOSED AS DIRECTED BY THE ENGINEER). REPLACE COLD-MILLED AREAS WITH DGAC IN LIFTS NOT TO EXCEED 4" IN DEPTH LEAVING 2" FOR OVERLAY. IN AREAS WHERE 5"-7" AC OVER 5"-20" AGGREGATE BASE OR SUBGRADE EXISTS, THE EXISTING PAVEMENT AND SUFFICIENT DEPTH OF EXISTING AGGREGATE BASE/SUBGRADE SHALL BE REMOVED TO BRING THE FINAL GRADE TO A DEPTH OF 6" BELOW THE EXISTING SURFACE. REPLACE THE REMOVED AC AREAS WITH 6" OF FULL DEPTH DGAC IN LIFTS NOT TO EXCEED 4" OF THICKNESS LEAVING 2" FOR OVERLAY AS DIRECTED BY THE ENGINEER.
- COLD MILL EXISTING A.C. PAVEMENT TO A MINIMUM UNIFORM DEPTH OF 2" AS SHOWN ON TYPICAL SECTION HEREON AND AS DIRECTED BY THE ENGINEER. COLD MILLING SHALL EXTEND TO CURB RETURNS OF ALL CROSS STREETS ALONG VICTORIA AVENUE.

SPECIFIC NOTES:

- CONTRACTOR SHALL REPLACE EXISTING STRIPING AND MARKINGS AFTER OVERLAY TO THE ORIGINAL CONFIGURATIONS, UNLESS THERE IS A SIGNING & STRIPING PLAN, IN ACCORDANCE WITH SPECIAL PROVISIONS.
- ADEQUATE FLOW LINES SHALL BE MAINTAINED AT ALL LOCATIONS.
- AT THE EXISTING WHEELCHAIR RAMPS WHETHER OR NOT THEY REQUIRE ALTERATION, THE CROSS SLOPE OF THE PAVEMENT ADJACENT TO THE RAMP SHALL NOT EXCEED 5% FOR ANY RAMP. WHERE SUCH PAVEMENT CROSS-SLOPE EXCEEDS 5%, CONTRACTOR MAY BE REQUIRED TO PERFORM ADDITIONAL COLD-MILLING TO SATISFY THE REQUIRED 5% SLOPE AND PROVIDE SMOOTH TRANSITION TO PAVEMENT IN THE DRIVING LANES.
- LOCATIONS FOR DIGOUTS SHALL BE MARKED ON SITE AT THE TIME OF CONSTRUCTION AND AFTER COLD-MILLING OPERATIONS HAVE BEEN COMPLETED, WHERE REQUIRED.
- FOR WHEELCHAIR RAMPS AT LOCATIONS WHERE SIDEWALK IS LESS THAN 6 FEET WIDE, CONTRACTOR SHALL INSTALL THE SPECIFIED CITY STANDARD WHEELCHAIR RAMP AND TRANSITION THE ASSOCIATED SIDEWALK TO MATCH EXISTING.
- DURING THE CONSTRUCTION WORK, ANY SEWER, STORM DRAIN & UTILITY MANHOLES AND WATER GATE VALVES IF NEEDED, SHALL BE RAMPED UP OR DOUBLE ADJUSTED TO FACILITATE THE COLD-MILLING AND PAVING OPERATIONS. FOLLOWING THE CONSTRUCTION OF A.R.H.M.-GG AC FINAL FINISH SURFACE, ALL MANHOLES AND GATE VALVE COVERS SHALL BE ADJUSTED TO GRADE AS NEEDED.



VICTORIA AVENUE
TYPICAL SECTION
COTTAGE ST. TO WOODBINE AVE
T.I.=8

<p>Underground Service Alert CALL BEFORE YOU DIG CALL TOLL FREE 1-800-227-2600 TWO WORKING DAYS BEFORE YOU DIG</p>	<p>ENGINEER IN RESPONSIBLE CHARGE <i>Ibrahim A. Massoud</i> IBRAHIM A. MASSOUD R.C.E. No. 64224 expires 6/30/11 DATE: 1/6/10</p>	<p>REGISTERED PROFESSIONAL ENGINEER IBRAHIM A. MASSOUD No. 64224 Exp. 6-30-2011 CIVIL STATE OF CALIFORNIA</p>	<p>DESIGNED BY: OS DRAWN BY: OS CHECKED BY: ADE</p>	<p>CITY OF RIVERSIDE, CALIFORNIA PUBLIC WORKS DEPARTMENT</p>	<p>VICTORIA AVENUE REHABILITATION FROM COTTAGE STREET TO WOODBINE STREET</p>	<p>ACCT. NO. 9589330-440302 W.D. NO. 0915625</p>
				<p>APPROVED BY: [Signature] DATE: 1/13/10</p>		<p>R-4151 SHEET 1 OF 1 FILE NO.</p>